



## WORK SESSION AGENDA

**PLANNING & ZONING COMMISSION WORK SESSION  
CITY OF LEANDER, TEXAS  
Pat Bryson Municipal Hall ~ 201 North Brushy Street  
Leander, Texas**

**Thursday ~ December 8, 2016 at 6:00 p.m.**

<b>Place 1 Laura Lantrip</b>	<b>Place 5 Vacant</b>
<b>Place 2 John Cosgrove</b>	<b>Place 6 Angela Means</b>
<b>Place 3 Jason Anderson – Vice Chair</b>	<b>Place 7 Marshall Hines</b>
<b>Place 4 Sid Sokol - Chair</b>	

*This meeting is open to the Public but does not allow for public participation  
No action will be taken by the Planning and Zoning Commissioners regarding City business before, during or  
after this meeting*

1. Open Work Session
2. Roll Call
3. Discussion of Ordinance Amendments Related to Comprehensive Plan Implementation
4. Transportation Plan Discussion
5. Adjournment

### **CERTIFICATION**

This meeting will be conducted pursuant to the Texas Government Code Section 551.001 et seq. At any time during the meeting the P & Z Commission reserves the right to adjourn into executive session on any of the above posted agenda items in accordance with the sections 551.071 [litigation and certain consultation with attorney], 551.072 [acquisition of interest in real property], 551.073 [contract for gift to city], 551.074 [certain personnel deliberations] or 551.076 [deployment/implementation of security personnel or devices]. The City of Leander is committed to compliance with the American with Disabilities Act. Reasonable modifications and equal access to communications will be provided upon request. Please call the City Secretary at (512) 528-2743 for information. Hearing impaired or speech disabled persons equipped with telecommunication devices for the deaf may call (512) 528-2800. I certify that the above agenda for this meeting of the P & Z Commission of the City of Leander, Texas, was posted on the bulletin board at City Hall in Leander, Texas on the 22nd day of July, 2016 by 5:00 pm pursuant to Chapter 551 of the Texas Government Code.

## **FY 2016-17 Comprehensive Plan Implementation Actions for Short- and Mid-Term Recommendations**

### **Goal - Enhance Leander's public spaces to create and link destinations**

**Recommendation** - Develop and implement streetscape designs for key corridors

**Action** - revise the zoning ordinance to create standard streetscape elements for major corridors and residential streets

**Recommendation** - Develop a corridor beautification strategy

**Action** - update zoning ordinance to create standard streetscape elements for major corridors

DRAFT

# COMPOSITE ZONING ORDINANCE AMENDMENTS

## ARTICLE III – USE COMPONENTS

### SECTION 6: SFL – SINGLE-FAMILY LIMITED

#### (a) Statement of Intent

~~(5) Lots that average less than forty five (45') feet in width along a block shall front on a street with a ROW of fifty six (56) feet or greater and a pavement width of thirty six (36) feet or greater unless access to garages on such lots is from an alley. All lots shall provide access to garages from a rear alley.~~

### SECTION 7: SFT – SINGLE-FAMILY TOWNHOUSE

#### (a) Statement of Intent

~~(5) Frontage for such lots shall be provided by a street with a ROW of fifty six (56) feet or greater and a pavement width of thirty six (36) feet or greater unless access to garages on such lots is from an alley. All lots shall provide access to garages from a rear alley.~~

## ARTICLE VI – SITE STANDARDS

### SECTION 1: LANDSCAPING, TREE PRESERVATION AND PROTECTION, AND SCREENING REQUIREMENTS

#### (b) Landscape Requirements.

(11) The following is for other than single-family or two-family development: Trees and shrubs identified on the preferred plant list ~~orand~~ included in the Grow Green Guide, shall be utilized within the required landscaping as described below. At least seventy five percent (75%) of the planted trees are required to be ~~significant-large trees/shade trees,- the remaining trees may be small trees or ornamental trees all of which shall be selected from the preferred plant list or Grow Green Guide. such as oak, elm, pecan, walnut, hickory, cherry, cypress, redbud and any rare species.~~ Existing significant trees and shrubs that are retained in healthy condition may count toward fulfillment of these requirements. In calculating the credit from existing significant trees and shrubs that are retained, shrubs shall be credited on a one for one basis if such shrub is equivalent or greater in size to a comparable five gallon container grown shrub. Trees shall be credited on a caliper inch basis (for every one caliper inch of a tree that is saved, credit shall be given for one caliper inch of a tree required to be planted), with saved significant trees over 18" caliper counting on a one for two basis (for every one caliper inch of a significant tree over 18" caliper that is saved, credit shall be given for two caliper inches of a tree required to be planted).

(i) For every six hundred (600) square feet of landscape area and setback area required by this ordinance, two (2) shade trees (two inch caliper or larger) and four (4) shrubs (five gallon container size or larger) shall be planted. Two ten gallon ornamental trees may be substituted for every one required shade tree as long as at least half of the required number of shade trees is installed.

(ii) To allow for larger landscaped activity areas at school facilities, for every 600 sq. ft. of landscaping required by this section, 1 tree and 3 shrubs (five gallon container size or larger) shall be planted.

~~(iii)~~ To reduce the thermal impact of unshaded parking lots, additional trees shall be planted as necessary so that the center point of each parking space is no more than 50 feet away from the trunk of a tree.

~~(iii)~~(iv) Street trees shall be planted between the sidewalk and back of curb (or edge of pavement) in accordance with the City's street tree detail. Street tree species shall be consistent on both sides of the street within a block, shall be a street tree species listed in the preferred plant list, and shall be specified in the Construction Plans for the subdivision (if applicable). Street tree spacing shall not exceed thirty (30) feet on center between trees on the same side of the street. Street trees may be counted toward the required tree plantings within the front or street side setback area.

(12) Minimum landscape requirements for detached single-family and two-family residential structures ~~shall be (except for townhouses on lots less than thirty five feet wide in the SFT district) shall be two (2) two-inch significant trees such as oak, elm, pecan, walnut, hickory, cherry, cypress, redbud and any rare species measured eighteen inches above finished grade immediately after planting, three (3) one-gallon shrubs, three (3) five-gallon shrubs and turf grass or an alternative material as defined in this section from the front property line to the front two (2) corners of the structure and a minimum coverage area extending 3' from the slab/foundation to protect water runoff from the roof drip line. If lawn grass is not used in this area, then rain gutter systems shall be in place. One three and a half inch caliper tree may be substituted for two (2) two-inch trees if the tree is planted in the front yard. Each single-family and two-family lot shall be required to plant at least two (2) trees measuring at least two (2) caliper inches selected from the City's preferred plant list or the Grow Green Guide. At least one of the required trees shall be a street tree planted between the sidewalk and the back of curb (or edge of pavement) in accordance with the City's street tree detail. Street tree species shall be consistent on both sides of the street within a block, shall be a street tree species listed in the preferred plant list, and shall be specified in the Construction Plans for the subdivision. Street tree spacing shall not exceed thirty (30) feet on center between trees on the same side of the street which may require two (2) or more street trees to be planted per lot to achieve the required spacing. Lots less than thirty (30) feet wide and with less than a fifteen (15) foot deep front yard are only required to plant one (1) tree which shall be the street tree. Corner lots shall plant street trees on both streets.~~ Existing trees and shrubs that are retained in healthy condition may count toward fulfillment of these requirements. ~~Minimum landscape requirements for townhouses constructed in the SFT district on lots less than thirty-five feet wide are the same as above except that only one hardwood tree is required to be planted per lot~~

(14) The landscaping shall be placed upon that portion of a tract or lot that is being developed. Fifty percent (50%) of the required landscaped area and required plantings contained in the landscape requirements listed in this section shall be installed between the front property lines and the building being constructed. Undeveloped portions of a tract or lot shall not be considered landscaped, except as specifically approved by the City. ~~Landscaping placed within public right-of ways~~

shall not fulfill the minimum landscape requirements.

**(f) Major Corridor Streetscape Standards.**

- (1) Major Corridors include all arterials identified on the City's Transportation Plan.
- (2) All development adjacent to a corridor shall install street trees between the back of curb or edge of pavement and the required sidewalk in accordance with the City's street tree detail. The trunk of the street tree shall be no closer than 3 feet from the back of curb or edge of pavement and no further than eight (8) feet; the Director of Planning may approve alternative locations if specific conditions warrant. Street tree species shall be consistent on both sides of the corridor, species may change at a street intersection. Street trees shall be spaced no more than thirty (30) feet on center and shall be in a straight line along the corridor.
- (3) If a parking lot is located between the right-of-way and a building along a corridor, the parking lot must be fully screened from view of the corridor to a height of four (4) feet with one or more of the following elements:
  - (a) a four (4) foot tall masonry screening wall located at or behind the required parking setback
  - (b) a berm with landscaping that measures at least four (4) feet tall at the top of the landscaping located within the required front setback area
- (4) In addition to other restrictions in this ordinance, the following items shall not be located between a primary building and the right-of-way along a major corridor:
  - (a) mechanical equipment other than that of a public utility
  - (b) drive-through service lanes or queuing spaces
  - (c) accessory structures
- (5) The following standards apply to the screening of the rear or service side of buildings along a Major Corridor:
  - (a) the rear or service side of a building may not face a Major Corridor within four hundred (400) feet of the corridor's right-of-way.
  - (b) loading areas and service drives must be screened from view of the Major Corridor with landscaping that is six (6) feet tall at installation or a wing wall that extends from the building that is a minimum of six (6) feet tall and constructed of the same or significantly similar materials as the primary building.
- (6) The following standards shall apply to all major intersections along the corridor:
  - (a) major intersections are those intersections with other major corridors or collector roads
  - (b) all four corners of a major intersection shall have a coordinated landscape design
  - (c) all four corners of a major intersection shall include identity monument signs which shall be coordinated and consistent in design and materials
  - (d) identity monument signs may include the name and/or logo of the adjacent development and shall include the City of Leander logo
- (7) Major Corridors shall comply with the following landscape and hardscape materials requirements
  - (a) each major corridor shall have a primary landscape and hardscape materials palette
  - (b) landscaping and hardscaping (including walls and planters) within the right-of-way and front setback of all properties along a major corridor shall comply with the corridor materials palette
  - (c) the corridor materials palette shall be maintained by the Planning Department
- (8) Walls constructed along Major Corridors shall meet the following requirements in addition to any other requirements of this ordinance:
  - (a) all walls constructed parallel to a major corridor shall comply with the corridor materials palette

- (b) walls that continue from one development or property to another within the same block shall continue the same material and design
- (c) wall material or design may change in a new block as long as the materials are consistent with the materials palette for the corridor

## ARTICLE VIII - ARCHITECTURAL STANDARDS

### SECTION 5: MASONRY AND OTHER ARCHITECTURAL STANDARDS

- (j) ~~Single-family and two-family~~ Residential dwelling street-facing garage standards. To prevent residential streetscapes from being dominated by garage doors, and to allow the visually interesting features of the house to dominate the streetscape, the following standards shall apply:
  - (1) All residential dwellings in the CH, NR, SFL and SFT districts or on any lot or building envelope less than fifty (50) feet wide shall provide access to garages from a rear alley.
  - (2) No more than 50% of residential dwellings within any final plat within the SFC, SFU and SFS districts may have front loaded, street facing garages forward of the rear wall of the primary structure.
  - (3) Three car garages in the SFC, SFU and SFS districts may only have two street facing garage doors.
  - (4) No residential dwellings within the SFE and SFR districts shall have front entry, street facing garages forward of the rear wall of the primary structure.
  - (5) Street facing garage doors shall be painted the same color as the primary color of the house.
  - (6) Residential dwellings with three garage doors in the SFC, SFU and SFS districts must utilize an alternative driveway paving material such as exposed aggregate, tinted concrete, brick pavers or similar surface material as approved by the Director of Planning.
  - (7) Residential driveway width within the right-of-way is limited to eighteen (18) feet.
  - ~~(1)(8)~~ Except as provided for in this subsection, street-facing garages must be recessed at least five (5) feet behind the ground floor living area of the dwelling or a roof-covered porch that is at least seven (7) feet wide by six (6) feet deep.
  - ~~(2)(9)~~ The total width of the garage door openings of a garage with street-facing doors shall be no more than fifty (50) percent of the ground floor street-facing linear building frontage.
  - ~~(3)(10)~~ Street-facing garages that have a total garage door opening width that is less than forty (40) percent of the ground floor street-facing linear building frontage width, may be in-line with the ground floor living area or roof-covered porch that is at least seven (7) feet wide by six (6) feet deep.
  - ~~(4)(11)~~ Street-facing garages that have a total garage door opening width of less than forty (40) percent of the ground floor street-facing linear building frontage width and that incorporate enhanced architectural features as described in this section, may protrude up to five (5) feet in front of the ground floor living area of the dwelling or roof-covered porch that is at least seven (7) feet wide by six (6) feet deep, but in no case shall the garage be setback from the street less than the minimum setback for the zoning district.
  - ~~(5) Street facing three car garages that have a total garage door opening width of less than fifty (50) percent of the ground floor street-facing linear building frontage width and that incorporate enhanced architectural features as described in this section, may have up to two (2) bays of the garage protrude up to five (5) feet in front of the ground floor living area of the dwelling or roof-covered porch that is at least seven (7) feet~~

~~wide by six (6) feet deep, but in no case shall the garage be setback from the street less than the minimum setback for the zoning district. The third bay of the garage shall be located in line with the ground floor living area of the dwelling or roof covered porch that is at least seven (7) feet wide by six (6) feet deep.~~

~~(6)~~(12) For garages that meet the standards of subsection ~~(4) or (5)~~(11) and where the site topography (typically slopes greater than 10% or for other similar topography as allowed by the Planning Director) requires stairs from the garage into the living space, the garage may protrude up to eight (8) feet in front of the ground floor living area of the dwelling or roof-covered porch that is at least seven (7) feet wide by six (6) feet deep, but in no case shall the garage be setback from the street less than the minimum building setback for the zoning district.

~~(7)~~(13) The Planning Director may approve garage placement that does not meet the standards of this section in the case of unique site conditions including the existence of significant trees, extreme topography and similar natural features.

~~(8)~~(14) For the purposes of this subsection, enhanced architectural features shall include:

- a. Architectural garage doors that are painted to match the color scheme of the house and include decorative hardware; or
- b. Doors that have a natural wood appearance; and
- c. Both a. and b. above must also be combined with at least one of the following features:
  - (i) a garage door recess of at least two (2) feet;
  - (ii) a roof overhang over the garage doors with supporting architectural columns that extends at least two (2) feet in front of the garage doors; or
  - (iii) any similar architectural feature, approved by the Planning Director, that diminishes the prominence of the garage doors on the street-facing building facade.

~~(9)~~(15) Garages accessed from rear alleys are exempt from the standards of this section.

~~(10)~~(16) Second or higher floor living areas do not count toward the measurement of ground floor street-facing linear building frontage.

## SUBDIVISION ORDINANCE AMENDMENTS

### SECTION 22. PRELIMINARY PLAT

#### (c) Content.

(3) Improvements.

(viii) The location and species of street trees for each street within the preliminary plat.

### SECTION 23. CONSTRUCTION PLANS

#### (c) Content.

(14) Landscaping and Screening. The location, size and description of all landscaping and screening materials as required by the Composite Zoning Ordinance.

~~(14)~~(15) Street Trees. The location and species of all required street trees on every street in accordance with all current City standards, specifications and criteria for the installation of street trees.

~~(15)~~(16) Design Criteria. Final design criteria, reports, calculations, and all other related computations, if not previously submitted with the Preliminary Plat.

~~(16)~~(17) Cost Estimates. A cost estimate of each required improvement, prepared, signed and sealed by a professional engineer licensed to practice in the State of Texas.

DRAFT

**Centers – Evoke a sense of place and have elements that contribute to a cohesive identity.**

**Recommendation** – Develop architectural standards for centers

**Action** - update the zoning ordinance to create standards for master architectural plans.

DRAFT

# COMPOSITE ZONING ORDINANCE AMENDMENTS

## ARTICLE VIII – ARCHITECTURAL STANDARDS

### SECTION 5 – MASONRY AND OTHER ARCHITECTURAL STANDARDS

- (j) Single-family and two-family residential dwelling street-facing garage standards. To prevent residential streetscapes from being dominated by garage doors, and to allow the visually interesting features of the house to dominate the streetscape, the following standards shall apply:
- (1) Except as provided for in this subsection, street-facing garages must be recessed at least five (5) feet behind the ground floor living area of the dwelling or a roof-covered porch that is at least seven (7) feet wide by six (6) feet deep.
  - (2) The total width of the garage door openings of a garage with street-facing doors shall be no more than fifty (50) percent of the ground floor street-facing linear building frontage.
  - (3) Street-facing garages that have a total garage door opening width that is less than forty (40) percent of the ground floor street-facing linear building frontage width, may be in-line with the ground floor living area or roof-covered porch that is at least seven (7) feet wide by six (6) feet deep.
  - (4) Street-facing garages that have a total garage door opening width of less than forty (40) percent of the ground floor street-facing linear building frontage width and that incorporate enhanced architectural features as described in this section, may protrude up to five (5) feet in front of the ground floor living area of the dwelling or roof-covered porch that is at least seven (7) feet wide by six (6) feet deep, but in no case shall the garage be setback from the street less than the minimum setback for the zoning district.
  - (5) Street-facing three car garages that have a total garage door opening width of less than fifty (50) percent of the ground floor street-facing linear building frontage width and that incorporate enhanced architectural features as described in this section, may have up to two (2) bays of the garage protrude up to five (5) feet in front of the ground floor living area of the dwelling or roof-covered porch that is at least seven (7) feet wide by six (6) feet deep, but in no case shall the garage be setback from the street less than the minimum setback for the zoning district. The third bay of the garage shall be located in-line with the ground floor living area of the dwelling or roof-covered porch that is at least seven (7) feet wide by six (6) feet deep.
  - (6) For garages that meet the standards of subsection (4) or (5) and where the site topography (typically slopes greater than 10% or for other similar topography as allowed by the Planning Director) requires stairs from the garage into the living space, the garage may protrude up to eight (8) feet in front of the ground floor living area of the dwelling or roof-covered porch that is at least seven (7) feet wide by six (6) feet deep, but in no case shall the garage be setback from the street less than the minimum building setback for the zoning district.
  - (7) The Planning Director may approve garage placement that does not meet the standards of this section in the case of unique site conditions including the existence of significant trees, extreme topography and similar natural features.
  - (8) For the purposes of this subsection, enhanced architectural features shall include:
    - a. Architectural garage doors that are painted to match the color scheme of the house and include decorative hardware; or
    - b. Doors that have a natural wood appearance; and
    - c. Both a. and b. above must also be combined with at least one of the following features:

- (i) a garage door recess of at least two (2) feet;
  - (ii) a roof overhang over the garage doors with supporting architectural columns that extends at least two (2) feet in front of the garage doors; or
  - (iii) any similar architectural feature, approved by the Planning Director, that diminishes the prominence of the garage doors on the street-facing building facade.
- (9) Garages accessed from rear alleys are exempt from the standards of this section.
- (10) Second or higher floor living areas do not count toward the measurement of ground floor street-facing linear building frontage.

(11) If new development is to occur within a neighborhood center, community center, or activity center, the development shall comply with the master architectural plan for that center. A master architectural plan is to be submitted to the Planning Department by the first party to develop within the center for administrative review and approval by the Planning Department. The master architectural plan shall be submitted prior to the submission of a building permit application and after being reviewed and approved shall be maintained on record with the Planning Department. The master architectural plan submitted to the Planning Department shall include common architectural elements that all future development will be required to match. These elements will include but are not limited to:

- a. Exterior building materials to be used on the exterior of each structure.
- b. Exterior building color palettes.
- c. Exterior elements of building facades such as wall accents, covered entries, columns, or other features.

Due to the nature of technological progression in building materials and ever changing building aesthetics, if a party demonstrates a bona fide attempt and intent to meet the standards of a master architectural plan but desires to use a different design aesthetic, alternative materials, different exterior elements on the building façade, or a color palette that differs from the original palette but is complementary to it then the party may do so if given approval by the Director of Planning.

## **Goal - Create strong neighborhoods with a variety of housing options**

**Recommendation** - Amend ordinances to accommodate a mixture of housing types

**Action** - Update the zoning and subdivision ordinance to provide for a mixture of housing types within neighborhoods. Revise the zoning ordinance to provide zoning categories for single-family detached condominiums and small scale multi-family products such as fourplexes, cottage courts, etc.

## **Goal - Continue to expand infrastructure to serve Leander residents**

**Recommendation** - Invest in utility planning that supports the land use pattern envisioned by the Future Land Use Map

**Action** - update the Water and Wastewater Master Plan, complete utility extension and oversizing projects

DRAFT

# COMPOSITE ZONING ORDINANCE AMENDMENTS

## ARTICLE I - GENERAL

### SECTION 6: DEFINITIONS

*Residential Areas and Residential Districts* mean one or more composite districts containing a residential component (SFR, SFE, SFS, SFU, SFC, SFL, CH, SFT, SFU/MH, TF, MF) or a PUD district approved for such uses.

*Tiny Home* means a single-family detached home that is between one hundred twenty (100) square feet and nine hundred (900) square feet in size. This home is a permanent structure that is constructed on or off site.

## ARTICLE II – ESTABLISHMENT OF ZONING REGULATIONS

### SECTION 2: ESTABLISHMENT OF ZONING DISTRICTS

#### (a) Zoning Districts

##### (1) Use Components

(i)	<b>SFR</b>	Single-Family Rural
(ii)	<b>SFE</b>	Single-Family Estate
(iii)	<b>SFS</b>	Single-Family Suburban
(iv)	<b>SFU</b>	Single-Family Urban
(v)	<b>SFC</b>	Single-Family Compact
<u>(vi)</u>	<u><b>SFL</b></u>	<u>Single-Family Limited</u>
<del>(vi)</del> (vii)	<u><b>CH</b></u>	<u>Cottage Housing</u>
<del>(vii)</del> (viii)	<b>SFT</b>	Single-Family Townhouse
<del>(viii)</del> (ix)	<b>SFU/MH</b>	Single-Family Urban, Manufactured Home
<u>(x)</u>	<u><b>TF</b></u>	<u>Two-Family</u>
<u>(xi)</u>	<u><b>TH</b></u>	<u>Tiny Home</u>
<u>(xii)</u>	<u><b>NR</b></u>	<u>Neighborhood Residential</u>
<del>(ix)</del> (xiii)	<b>MF</b>	Multi-Family
<del>(x)</del> (xiv)	<b>LO</b>	Local Office
<del>(xi)</del> (xv)	<b>LC</b>	Local Commercial
<del>(xii)</del> (xvi)	<b>GC</b>	General Commercial
<del>(xiii)</del> (xvii)	<b>HC</b>	Heavy Commercial
<del>(xiv)</del> (xviii)	<b>HI</b>	Heavy Industrial
<del>(xv)</del> (xix)	<b>PUD</b>	Planned Unit Development

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(b) The following table illustrates the allowable combining options for composite districts. There are over one hundred possible combining options. If an option is not shown as permitted (P), that option is not-permitted.

**Composite District Combining Options (P – Permitted)**

Use Component	Site Component					Arch. Component			
	1	2	3	4	5	A	B	C	D
SFR	P	P	P			P	P		
SFE	P	P	P			P	P		
SFS	P	P	P			P	P		
SFU	P	P	P			P	P		
SFC	P	P	P			P	P		
SFL	P	P	P			P	P		
<u>CH</u>	<u>P</u>	<u>P</u>	<u>P</u>			<u>P</u>	<u>P</u>		
SFT	P	P	P			P	P		
SFU/MH	P	P	P			P	P		
TF	P	P	P			P	P		
<u>TH</u>	<u>P</u>	<u>P</u>	<u>P</u>			<u>P</u>	<u>P</u>		
<u>NR</u>	<u>P</u>	<u>P</u>	<u>P</u>			<u>P</u>	<u>P</u>		
MF	P	P	P			P	P		
LO	P	P	P			P	P	P	
LC	P	P	P			P	P	P	
GC	P	P	P	P	P	P	P	P	P
HC	P	P	P	P	P	P	P	P	P
HI	P	P	P	P	P	P	P	P	P

**ARTICLE III – USE COMPONENTS**

**SECTION 1: SFR – SINGLE-FAMILY RURAL**

**(a) Statement of Intent**

The Single-Family Rural use component is intended for the development of single-family detached dwellings on lots one acre or larger in size and for other uses that are compatible and complimentary to large lot and very low density residential development. The purpose of this component is to provide regulations to preserve rural character and maintain and protect the City's single-family residences and neighborhoods in an area with larger lot sizes. This component is also intended to preserve the larger tracts of land for future economic development in accordance with the Comprehensive Plan, while permitting rural/agricultural uses on the land to continue. In addition, this use component is appropriate for areas where sewer is not feasible, areas with steep topography, floodplain, or other natural features where there are strict regulations on wastewater.

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**SECTION 3: SFS – SINGLE-FAMILY SUBURBAN**

**(c) Lot Size Minimum**

Interior Lots				Corner Lots			
Area	<u>Min Width</u>	<u>Max Width</u>	Depth	Area	<u>Min Width</u>	<u>Max Width</u>	Depth
9,000 sq. ft.	70'	<u>80'</u>	115'	10,000 sq. ft.	80'	<u>90'</u>	115'

- (1) Up to ten percent (10%) of lots in any final plat may have widths, depths and/or areas up to ten percent (10%) greater than or less than requirements.
- (2) In addition, for every one foot that a lot is wider or deeper than required, or for every one square foot that a lot is larger than required, another lot may be reduced by the equivalent amount, for up to ten percent (10%) of the lots in any final plat being allowed to have such reduced sizes, and up to ten percent (10%) less than requirements. The applicant may increase these prescribed limits to up to double the amounts if the applicant can demonstrate increased tree or other significant natural feature protection.

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**SECTION 4: SFU – SINGLE-FAMILY URBAN**

**(a) Statement of Intent**

The Single-Family Urban use component provides for the development of single-family detached dwellings on moderate urban standard sized lots and for other compatible and complimentary uses. The purpose of this component is to provide regulations to maintain and protect the City's single-family residences and neighborhoods in areas with moderate lot sizes. Such components are generally intended to offer variety in housing opportunities and in the fabric of the neighborhoods. In addition, a variety of housing types shall be provided within one half mile of intersections of major intersections such as arterials or collectors. This component provides moderate size lots that may serve as a transition between larger lots and higher density areas.

**(b) Conforming Uses**

A building or premise shall be used only for the following purposes:

- (1) Any use permitted in the "SFS" – Single-Family Suburban component except that an accessory dwelling is only permitted with a Type 3 site component unless such lot meets the standards of the SFS or SFE district.
- (2) Single-family dwelling of not less than twelve hundred (1,200) square feet of living area

**(c) Lot Size Minimum**

Interior Lots				Corner Lots			
Area	<u>Min</u> Width	<u>Max</u> Width	Depth	Area	<u>Min</u> Width	<u>Max</u> Width	Depth
7,200 sq. ft.	60'	<u>70'</u>	110'	8,000 sq. ft.	70'	<u>80'</u>	110'

- (1) Up to ten percent (10%) of lots in any final plat may have widths, depths and/or areas up to ten percent (10%) greater than or less than requirements.
- (2) In addition, for every one foot that a lot is wider or deeper than required, or for every one square foot that a lot is larger than required, another lot may be reduced by the equivalent amount, for up to ten percent (10%) of the lots in any final plat being allowed to have such reduced sizes, and up to ten percent (10%) less than requirements. The applicant may increase these prescribed limits to up to double the amounts if the applicant can demonstrate increased tree or other significant natural feature protection.

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**SECTION 5: SFC – SINGLE-FAMILY COMPACT**

**(a) Statement of Intent**

The Single-Family Compact use component provides for the development of single-family detached dwellings on small lots and for other compatible and complimentary uses. The purpose of this component is to provide regulations to maintain and protect the City's single-family residences and neighborhoods in areas with small lot sizes. Such components are generally intended to offer variety in housing opportunities and in the fabric of the neighborhoods, and to be developed on a moderate scale with a maximum district size of seventy-five (75) acres. A variety of housing types shall be provided within one half mile of intersections of major intersections such as arterials or collectors and along residential collectors. The higher density residential shall be located closest to the intersections of major intersections such as arterials or collectors and transition to lower density uses further away from the major intersections. This component provides for higher density lots and serves as a transition between moderate size lots and higher density areas.

**(b) Conforming Uses**

A building or premise shall be used only for the following purposes:

- (1) Any use permitted in the "SFU" – Single-Family Urban component
- (2) Single-family dwelling of not less than eleven hundred (1,100) square feet of living area

**(c) Lot Size **Minimum****

Interior Lots				Corner Lots			
Area	Min Width	Max Width	Depth	Area	Min Width	Max Width	Depth
5,500 sq. ft.	50'	60'	105'	6,500 sq. ft.	60'	70'	105'

- (1) Up to ten percent (10%) of lots in any final plat may have widths, depths and/or areas up to ten percent (10%) greater than or less than requirements.
- (2) In addition, for every one foot that a lot is wider or deeper than required, or for every one square foot that a lot is larger than required, another lot may be reduced by the equivalent amount, for up to ten percent (10%) of the lots in any final plat being allowed to have such reduced sizes, and up to ten percent (10%) less than requirements. The applicant may increase these prescribed limits to up to double the amounts if the applicant can demonstrate increased tree or other significant natural feature protection.

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**SECTION 6: SFL – SINGLE-FAMILY LIMITED**

**(a) Statement of Intent**

The Single-Family Limited use component provides for the development of single-family detached dwellings on small lots, including zero lot line development, and for other compatible and complimentary uses. The purpose of this component is to provide regulations to maintain and protect the City's single-family residences and neighborhoods in areas where it is appropriate to have small lot sizes and reduced setbacks. A variety of housing types shall be provided within one half mile of intersections of major intersections such as arterials or collectors and along residential collectors. The higher density residential shall be located closest to the intersections of major intersections such as arterials or collectors and transition to lower density uses further away from the major intersections. This component provides for higher density lots and serves as a transition between moderate size lots and higher density areas. This component should be located closer major intersection. This component is generally intended as follows:

- (1) To provide an orderly transition to and create a buffer between larger lot neighborhoods and more intensive uses such as multi-family or commercial uses or arterial roadways.

- (2) To create more variety in housing opportunities in the fabric of neighborhoods.
- (3) To be located in planned developments of greater than 100 acres and comprising less than twenty percent (20%) of the lots, or to provide infill opportunities in appropriate areas of the City.
- (4) To include or be located within six hundred feet of parkland or other recreational open space and/or transit opportunities.
- (5) ~~All lots shall be accessed from a rear alley. Garages shall not face the street. Lots that average less than forty five (45') feet in width along a block shall front on a street with a ROW of fifty six (56) feet or greater and a pavement width of thirty six (36) feet or greater unless access to garages on such lots is from an alley.~~
- (6) Proposals for lots less than forty-one (41') feet wide require PUD zoning as stated in Article II, Section 2 (e) (5) (iii).

**(b) Conforming Uses**

A building or premise shall be used only for the following purposes:

- (1) Any use permitted in the "SFC" – Single-Family Compact component
- (2) Single-family dwelling of not less than one thousand (1,050) square feet of living area

**(c) Lot Size Minimum**

<u>Interior Lots</u>				<u>Corner Lots</u>			
Area	<u>Min Width</u>	<u>Max Width</u>	Depth	Area	<u>Min Width</u>	<u>Max Width</u>	Depth
4,100 sq. ft.	41'	<u>50'</u>	100'	5,100 sq. ft.	51'	<u>60'</u>	100'

- (1) Up to ten percent (10%) of lots in any final plat may have widths, depths and/or areas up to ten percent (10%) greater than or less than requirements.
- (2) In addition, for every one foot that a lot is wider or deeper than required, or for every one square foot that a lot is larger than required, another lot may be reduced by the equivalent amount, for up to ten percent (10%) of the lots in any final plat being allowed to have such reduced sizes, and up to ten percent (10%) less than requirements. The applicant may increase these prescribed limits to up to double the amounts if the applicant can demonstrate increased tree or other significant natural feature protection.

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**SECTION 7: SFT – SINGLE-FAMILY TOWNHOUSE**

**(a) Statement of Intent**

The Single-Family Townhouse use component provides for the development of single-family attached dwellings on very small sized lots and for other uses that are compatible and complimentary to attached residential development. A variety of housing types shall be provided within one half mile of intersections of major intersections such as arterials or collectors and along residential collectors. The higher density residential shall be located closest to the intersections of major intersections such as arterials or collectors and transition to lower density uses further away from the major intersections. This component provides for higher density lots and serves as a transition between moderate size lots and higher density areas. -This component is generally intended as follows:

- (1) To provide an orderly transition and serve as a buffer between larger lot neighborhoods and more intensive uses such as multi-family or commercial uses or arterial roadways.
- (2) To create more variety in housing opportunities and in the fabric of the neighborhoods.
- (3) To include or be located within six hundred feet of parkland or other recreational open space.

- (4) To be located in planned communities of greater than 100 acres and comprising less than ten percent (10%) of the lots, or to provide infill opportunities in appropriate areas of the City such as in areas under transition.
- (5) Frontage for such lots shall be provided by a street with a ROW of fifty-six (56) feet or greater and a pavement width of thirty-six (36) feet or greater unless access to garages on such lots is from an alley.

**(b) Conforming Uses**

A building or premise shall be used only for the following purposes:

- (1) Any use permitted in the "SFL" – Single-Family Limited component,
- (2) Single-family dwelling of not less than nine hundred (900) square feet of living area.

**(c) Lot Size Minimum**

Interior Lots				Corner Lots			
Area	<u>Min Width</u>	<u>Max Width</u>	Depth	Area	<u>Min Width</u>	<u>Max Width</u>	Depth
2,000 sq. ft.	20'	<u>40'</u>	90'	3,500 sq. ft.	35'	<u>51'</u>	90'

- (1) Up to ten percent (10%) of lots in any final plat may have widths, depths and/or areas up to ten percent (10%) greater than or less than requirements.
- (2) In addition, for every one foot that a lot is wider or deeper than required, or for every one square foot that a lot is larger than required, another lot may be reduced by the equivalent amount, for up to ten percent (10%) of the lots in any final plat being allowed to have such reduced sizes, and up to ten percent (10%) less than requirements. The applicant may increase these prescribed limits to up to double the amounts if the applicant can demonstrate increased tree or other significant natural feature protection.

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**SECTION 8: SFU/MH – SINGLE-FAMILY URBAN, MANUFACTURED HOME**

**(a) Statement of Intent**

The Single-Family Urban, Manufactured Home component provides for the development of single-family homes and manufactured homes on moderate sized lots and for other uses that are compatible and complimentary to such uses on moderate sized lots. Such components are also intended to create more variety in housing opportunities. In addition, a variety of housing types shall be provided within one half mile of intersections of major intersections such as arterials or collectors. This component provides moderate size lots that may serve as a transition between larger lots and higher density areas.

**(b) Conforming Uses**

A building or premise shall be used only for the following purposes:

- (1) Any use permitted in the "SFU" Single-Family Urban component
- (2) Existing mobile homes, provided that no mobile home shall be hereafter placed or installed on any lot, tract or parcel of land within the City.
- (3) Manufactured homes (they are exempt from architectural component requirements and architectural standards except as required in this section )
- (4) Municipal buildings
- (4)(5) Tiny Homes in compliance with Section 11 of this ordinance; one (1) Tiny Home per lot or multiple Tiny Homes per lot as long as the number of homes does not exceed fixture count based on the meter size.

(c) Lot Size **Minimum**

Interior Lots				Corner Lots			
Area	Min Width	Max Width	Depth	Area	Min Width	Max Width	Depth
7,200 sq. ft.	60'	70'	110'	8,000 sq. ft.	70'	80'	110'

- (1) Up to ten percent (10%) of lots in any final plat may have widths, depths and/or areas up to ten percent (10%) greater than or less than requirements.
- (2) In addition, for every one foot that a lot is wider or deeper than required, or for every one square foot that a lot is larger than required, another lot may be reduced by the equivalent amount, for up to ten percent (10%) of the lots in any final plat being allowed to have such reduced sizes, and up to ten percent (10%) less than requirements. The applicant may increase these prescribed limits to up to double the amounts if the applicant can demonstrate increased tree or other significant natural feature protection.

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**SECTION 9: TF – TWO-FAMILY**

(a) **Statement of Intent**

The Two-Family use component provides for the development of two-family dwelling structures on intermediate sized lots and for other uses that are compatible and complimentary to intermediate sized lots and two-family dwellings. Such components are generally intended to provide an orderly transition and serve as a buffer between larger lot neighborhoods and more intensive uses and to create more variety in housing opportunities and in the fabric of the neighborhoods. The goal is to avoid more than ten acres of contiguous land having a two-family component. This component should include or be located within six hundred feet of parkland or other recreational open space. To avoid street congestion due to additional on-street parking, access to lots shall be provided by a street with a ROW of fifty-six (56) feet or greater and a pavement width of thirty-six (36) feet or greater unless lots average at least one hundred feet in width or unless garage access is from an alley. A variety of housing types shall be provided within one half mile of intersections of major intersections such as arterials or collectors and along residential collectors. The higher density residential shall be located closest to the intersections of major intersections such as arterials or collectors and transition to lower density uses further away from the major intersections. This component provides for higher density lots and serves as a transition between moderate size lots and higher density areas.

(b) **Conforming Uses**

A building or premise shall be used only for the following purposes:

- (1) Any use permitted in the "SFS" – Single-Family Suburban component
- (2) One or two single-family dwellings with a minimum living area of twelve hundred (1,200) square feet per dwelling.
- (3) Two (2) family dwellings, minimum living area - nine hundred (900) square feet per dwelling.

(c) **Lot Size**

Interior Lots				Corner Lots			
Area	Min Width	Max Width	Depth	Area	Min Width	Max Width	Depth
9,000 sq. ft.	70'	80'	115'	10,000 sq. ft.	80'	90'	115'

- (1) Up to ten percent (10%) of lots in any final plat may have widths, depths and/or areas up to ten percent (10%) greater than or less than requirements.
- (2) In addition, for every one foot that a lot is wider or deeper than required, or for every one square foot that a lot is larger than required, another lot may be reduced by the equivalent amount, for up to ten percent (10%) of the lots in any final plat being allowed to have such reduced sizes, and up to ten percent (10%) less than requirements. The applicant may increase these prescribed limits to up to double the amounts if the applicant can demonstrate increased tree or other significant natural feature protection.

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## **SECTION 10: CH – COTTAGE HOUSING**

### **(a) Statement of Intent**

The Cottage Housing use component provides for the development of multiple single-family detached dwellings on one lot and for other compatible and complimentary uses. The purpose of this component is to provide regulations to maintain and protect the City's single-family residences and neighborhoods in areas where it is appropriate to have cottage style development. This use component provides for greater flexibility in designing and placing structures while preserving open space, trees, and shared common areas. A variety of housing types shall be provided within one half mile of intersections of major intersections such as arterials or collectors and along residential collectors. The higher density residential shall be located closest to the intersections of major intersections such as arterials or collectors and transition to lower density uses further away from the major intersections. This component provides for higher density lots and serves as a transition between moderate size lots and higher density areas. This component is generally intended as follows:

- (1) To provide an orderly transition to and create a buffer between larger lot neighborhoods and more intensive uses such as multi-family or commercial uses or arterial roadways.
- (2) To create more variety in housing opportunities in the fabric of neighborhoods.
- (3) To include or be located within six hundred (600') feet of shared amenity space.

### **(b) Conforming Uses**

A building or premise shall be used only for the following purposes:

- (1) Three (3) or more detached single-family dwellings per lot with a limit of eight (8) dwellings per acre. The minimum dwelling size shall be no less than one thousand (1,050) square feet of living area.

### **(c) Development Design Standards**

- (1) Any condominium development shall have a common maintenance or ownership agreement shall be required to file all covenants and declarations governing those agreements at the time of platting. When property owner associations are established, membership shall be mandatory. The final plat shall reflect the Volume and Page reference of such covenants and declarations filing. No certificate of occupancy shall be issued for any dwelling designed to be constructed with a common wall without written verification that restrictive covenants have been filed with the county clerk establishing the requirement of a mandatory homeowners association and in the case of condominiums, a mandatory condominium regime, which includes all of the lots and improvements within the development. The mandatory homeowners association shall maintain the common landscaped areas, fences, all structures and other improvements on

the site owned in common, including the driveways, amenities, and any common parking areas and garages, and exterior maintenance of the principal structures.

**(d) Enclosed Garage and Parking**

- (1) Dwelling units with three or more bedrooms: A minimum of two garage-enclosed parking spaces and two additional off-street parking spaces (driveway may be counted toward provision of off-street parking) shall be provided per dwelling.
- (2) Dwelling units with two or fewer bedrooms: A minimum of one garage-enclosed parking space and two additional off-street parking spaces (driveway may be counted toward provision of off-street parking) shall be provided for each dwelling.
- (3) Parking for other uses shall be provided in accordance with Art. VI, Sec. 3.
- (4) Private drives shall constitute streets for the purposes of the application of Art. VIII Sec. 5 (j).

**(e) Additional Requirements:** (Each of the uses denoted herein shall conform to the following development standards. Note: A site component – Type 1, 2 or 3 - and an architectural component – Type A or B – must be combined with this use component.)

- (1) Use Standards (Article IV)
- (2) Site Components (Article V)
- (3) Site Standards (Article VI)
- (4) Architectural Components (Article VII)
- (5) Architectural Standards (Article VIII)

**SECTION 11: TH – TINY HOME**

**(a) Statement of Intent**

The Tiny Home use component provides for the development of smaller detached single-family homes on one lot and for other compatible and complimentary uses. The purpose of this component is to provide regulations to maintain and protect the City's single-family residences and neighborhoods in areas where it is appropriate to have tiny homes. This use component provides for greater flexibility in the size of homes. A variety of housing types shall be provided within one half mile of intersections of major intersections such as arterials or collectors and along residential collectors. The higher density residential shall be located closest to the intersections of major intersections such as arterials or collectors and transition to lower density uses further away from the major intersections. This component provides for higher density lots and serves as a transition between moderate size lots and higher density areas. This component is generally intended as follows:

- (1) To provide an orderly transition to and create a buffer between larger lot neighborhoods and more intensive uses such as multi-family or commercial uses or arterial roadways.
- (2) To create more variety in housing opportunities in the fabric of neighborhoods.
- (3) To include or be located within six hundred feet of a common amenity area.
- (4) To be located in planned developments of greater than twenty-five (25) acres and comprising less than twenty percent (20%) of the lots, or to provide infill opportunities in appropriate areas of the City.
- (5) To be located in close proximity to community services including, but not limited to grocery stores, restaurants, laundromats, etc.

**(b) Conforming Uses**

A building or premise shall be used only for the following purposes:

- (1) One (1) Tiny Home per lot. The minimum dwelling size shall be no less than one hundred (100) square feet and no more than five hundred (500) square feet of living area.

- (2) Multiple Tiny Homes per lot as long as the number of homes does not exceed fixture count based on the meter size.

**(c) Development Design Standards**

- (1) Tiny Homes shall have a minimum of one hundred (100) square feet of living area and no more than five hundred (500) square feet.
- (2) Tiny Homes shall be permanent structures that are constructed on or off site and shall be mounted to a permanent foundation.
- (3) Tiny Homes shall be installed in accordance with the following criteria:
- (i) Each Tiny Home is required to have a driveway and an address.
  - (ii) Garage and carport additions are permitted, provided they cover a paved parking area and are connected to a street by a paved drive, meet the minimum building setback requirements, and have roof and siding material compatible with the primary structure.
  - (iii) Patio and porch covers are permitted, provided they cover an improved patio, deck or porch and meet the minimum building setback requirements.
  - (iv) Each Tiny Home shall be connected to City Utilities including water and wastewater.
  - (v) Tiny Homes may be constructed of any materials compliant with the Type B Architectural Component or cementitious siding.
- (4) Multiple Tiny Homes on one (1) lot shall comply with the above and the following:
- (i) A site plan shall be submitted demonstrating compliance with building codes and the Composite Zoning Ordinance.
  - ~~(+)(ii)~~ Each home shall have a driveway access off of public ROW, a private street, and/or a private drive. All access is required to be paved with an all-weather surface.
  - ~~(ii)~~ The total number of Tiny Homes per property does not exceed fixture count based on the meter size.
  - (iii) Any condominium development shall have a common maintenance or ownership agreement shall be required to file all covenants and declarations governing those agreements at the time of platting. When property owner associations are established, membership shall be mandatory. The final plat shall reflect the Volume and Page reference of such covenants and declarations filing. No certificate of occupancy shall be issued for any dwelling designed to be constructed with a common wall without written verification that restrictive covenants have been filed with the county clerk establishing the requirement of a mandatory homeowners association and in the case of condominiums, a mandatory condominium regime, which includes all of the lots and improvements within the development. The mandatory homeowners association shall maintain the common landscaped areas, fences, all structures and other improvements on the site owned in common, including the driveways, amenities, and any common parking areas and garages, and exterior maintenance of the principal structures.
- (5) A minimum of two hundred (200) square feet of exterior open space is required per unit. This space must be a minimum of ten (10') feet wide.

**(d) Parking**

- (1) A minimum of one off-street parking spaces (driveway may be counted toward provision of off-street parking) shall be provided for each dwelling.
- (2) Parking for other uses shall be provided in accordance with Art. VI, Sec. 3.

**(f) Additional Requirements:** (Each of the uses denoted herein shall conform to the following development standards. Note: A site component – Type 1, 2 or 3 - and an architectural component – Type A or B – must be combined with this use component.)

- (1) Use Standards (Article IV)
- (2) Site Components (Article V)
- (3) Site Standards (Article VI)
- (4) Architectural Components (Article VII)
- (5) Architectural Standards (Article VIII)

**SECTION 12: NR – NEIGHBORHOOD RESIDENTIAL**

**(a) Statement of Intent**

The Neighborhood Residential use component provides for the development of small scale multi-family attached or detached dwellings on one lot and for other compatible and complimentary uses. The purpose of this component is to provide regulations to maintain and protect the City's single-family residences and neighborhoods in areas where it is appropriate to have higher density residential development. This use component allows for a variety of single-family and/or multi-family dwellings with gross densities based on the form of the building. The design of multifamily development, including building appearance, location of parking, setbacks, and landscaping, should be complementary to the existing neighborhood and mix of dwelling types. This district may be applied to existing neighborhoods to create a transition between single and two-family areas and higher density mixed residential or multi-family areas. A variety of housing types shall be provided within one half mile of intersections of major intersections such as arterials or collectors and along residential collectors. The higher density residential shall be located closest to the intersections of major intersections such as arterials or collectors and transition to lower density uses further away from the major intersections. This component provides for higher density lots and serves as a transition between moderate size lots and higher density areas. This component is generally intended as follows:

- (1) To provide an orderly transition to and create a buffer between single-family and two-family areas and more intensive uses such as multi-family or commercial uses or arterial roadways.
- (2) To create more variety in housing opportunities in the fabric of neighborhoods.
- (3) To include or be located within six hundred (600') feet of parkland or other recreational open space and/or transit opportunities.
- (4) To be located in planned developments of greater than 100 acres and comprising less than twenty percent (20%) of the lots, or to provide infill opportunities in appropriate areas of the City.

**(b) Conforming Uses**

A building or premise shall be used only for the following purposes:

- (1) One (1) structure with three (3) or more dwellings on one (1) lot or tract.
- (2) Tiny Homes in compliance with Section 11 of this ordinance; one (1) Tiny Home per lot or multiple Tiny Homes per lot as long as the number of homes complies with fixture count associated with one (1) LUE.

**(c) Lot Size**

<u>Interior Lots</u>			<u>Corner Lots</u>		
<u>Area</u>	<u>Min Width</u>	<u>Depth</u>	<u>Area</u>	<u>Min Width</u>	<u>Depth</u>
<u>9,000 sq. ft.</u>	<u>70'</u>	<u>115'</u>	<u>10,000 sq. ft.</u>	<u>80'</u>	<u>115'</u>

- (1) The maximum lot size shall not exceed two (2) times the lot size of any adjacent single-family or two-family lots.
- (2) Up to ten percent (10%) of lots in any final plat may have widths, depths and/or areas up to ten percent (10%) greater than or less than requirements.
- (3) In addition, for every one foot that a lot is wider or deeper than required, or for every one square foot that a lot is larger than required, another lot may be reduced by the equivalent amount, for up to ten percent (10%) of the lots in any final plat being allowed to have such reduced sizes, and up to ten percent (10%) less than requirements. The applicant may increase these prescribed limits to up to double the amounts if the applicant can demonstrate increased tree or other significant natural feature protection.

**(c) Development Design Standards**

- (1) The building height shall not exceed the permitted height any adjacent single-family or two-family development.
- (2) Any condominium development shall have a common maintenance or ownership agreement shall be required to file all covenants and declarations governing those agreements at the time of platting. When property owner associations are established, membership shall be mandatory. The final plat shall reflect the Volume and Page reference of such covenants and declarations filing. No certificate of occupancy shall be issued for any dwelling designed to be constructed with a common wall without written verification that restrictive covenants have been filed with the county clerk establishing the requirement of a mandatory homeowners association and in the case of condominiums, a mandatory condominium regime, which includes all of the lots and improvements within the development. The mandatory homeowners association shall maintain the common landscaped areas, fences, all structures and other improvements on the site owned in common, including the driveways, amenities, and any common parking areas and garages, and exterior maintenance of the principal structures.

**(d) Enclosed Garage and Parking**

- (1) Pending...

**(e) Additional Requirements:** (Each of the uses denoted herein shall conform to the following development standards. Note: A site component – Type 1, 2 or 3 - and an architectural component – Type A or B – must be combined with this use component.)

- (1) Use Standards (Article IV)
- (2) Site Components (Article V)
- (3) Site Standards (Article VI)
- (4) Architectural Components (Article VII)
- (5) Architectural Standards (Article VIII)

**SECTION 17: USE MATRIX**

Certain uses listed also need to be supported by an appropriate site component in order to be permitted. The uses set forth in this Use Matrix must also comply with any provisions governing that use set forth in Article III and Article V. In the event of a conflict between this Use Matrix and P = Permitted and S = Special Use Permit Required

Use	USE MATRIX																	
	SFR	SFE	SFS	SFU	SFC	SFL	CH	TH	NR	SFT	SFU/MH	TF	MF	LO	LC	GC	HC	HI
Amenity Center	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P
Apartment													P					
Artisan & Handcraft Activities														P	P	P	P	P

Assisted Living or Nursing Home																	P	P	P
Bar, Nightclub or Private Club																	P	P	P
Bank																	P	P	P
Bed and Breakfast																	P	P	P
Bingo																		P	P
Car Title Loan Shop																		P	P
Carwash																	P	P	P
Colleges, Universities, Vocational Schools, Higher Learning Institution																	P	P	P
Commercial parking																		P	P
Commercial Laundry																		P	P
Community Service including Community Center, Civic Organization, and cemetery/mausoleum (not including cremator or embalming)	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P
Contractor & Building Material Storage																			P
Cultural Facilities																		P	P
Day Care for 6 or fewer children	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P
Day Care, all other																	P	P	P
Entertainment activities																		P	P
Florist Shop																		P	P
Funeral Home, No Embalming or Cremation																		P	P
Funeral Home, Including Embalming & Cremation																		P	P
Golf course/country club	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P
Grocery Store																		P	P
Group Home Class 1	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P
Group Home Class 2																		P	P
Group Home Class 3																	S	S	
Home Occupations	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P
Hooka Lounge																			P
Hospital																		P	P
Hotel / Motel, Boarding Houses																		P	P
Liquor Store																		P	P
Major equipment sales and leasing																		P	P
Manufacture, Assembly or Processing of Materials of Non Hazardous Materials																			P
Manufactured Homes												P							
Manufactured Housing Sales																		P	P
Mixed Use/ Apartment																	P	P	P
Office																	P	P	P
Office, Medical																	P	P	P
Office/ Warehouse																		P	P
Outdoor Processing of Materials																			P
Park and related facilities	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P
Passenger terminal																			P

Personal Services (Barber Shop, Beauty Shop, Tailoring, Dressmaking, Shoe Repair, Small Appliance Repair, Bicycle Repair, Retail Bakeries, Catering, Dry Cleaning)																		P	P	P	P	
Pet Shop																			P	P	P	P
Pharmacy																		P	P	P	P	
Place of Worship	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	
Public Buildings	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	
Medical Clinic																		P	P	P	P	
Pawn Shop																				P	P	
Payday Lending																				P	P	
Real Estate Sales Office during the development and sale of a residential subdivision	P	P	P	P	P	P	P	P	P	P	P	P	P									
Restaurants																		P	P	P	P	
Research, Testing & Development Laboratory																			P	P	P	
Retail Sales of New Products and Service																		P	P	P	P	
Retail Sales, Second Hand Resale and Services																				P	P	
RV, Trailer, Commercial Motor Vehicle, or Boat Outdoor Storage																				P	P	
Schools, public or private including all levels up to high school	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	
Self-service storage/Mini-Warehouse																				P	P	
Single-Family, Attached										P												
Single-Family, Detached	P	P	P	P	P	P				P	P	P										
Single-family, zero lot line					P	P																
Stand Alone Crematory or Embalming Establishment																				P	P	
Tattoo and/or Body Piercing																				P	P	
Townhouse										P												
Two-Family Dwelling																P						
Utility, Minor	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	
Utility, Major																				P	P	
Vehicle Sales (New), Rental, Repair																			P	P	P	
Vehicle Sales (New & Used), Rental, Repair																				P	P	
Veterinarian Office																P	P	P	P	P	P	
Veterinarian Office, Animal Hospital or Animal Boarding																			P	P	P	
Warehouse and Distribution																				P	P	
Wholesale Activities																				P	P	
Wireless Communications - Stealth	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	P	
Wood yard																				P	P	



- (ii) At least eighty-five percent (85%) of the units are required to have at least one enclosed garage parking space and such garages are required to be leased, rented or sold with the applicable units.
  - (iii) Buildings shall be constructed to a residential scale. No first floor gross building area shall exceed 7,000 gross square feet. No building shall contain more than 14,000 gross square feet of floor area. No building shall be constructed to a height of greater than thirty-five (35) feet.
  - (iv) The front of all buildings shall face a public street or an exclusive front yard area for that building (not shared with another building) no less than twenty feet in depth and garage doors shall not be located on the front of the building.
  - (v) Front door access is shared so that a maximum of two front doors are provided with each building.
  - (vi) Each residential structure is designed to appear to be one large single-family structure (mansion).
  - (vii) Parking areas shall be no wider than two parking modules wide.
  - (viii) Dumpster enclosures, food waste containers, grease traps, outdoor seating associated with restaurants, loading areas, or similar improvements are not permitted within seventy-five (75) feet of a residential district unless such district is utilized for a non-residential use.
- (6) Residential Development in the SFT and SFL districts
- (i) Alley access to garages shall be provided for all lots in blocks where lots average less than fifty feet wide.
  - (ii) Where alleys are provided, it is encouraged that visually permeable fencing (e.g. picket or tubular metal fencing) is provided adjacent to such alleys and that the height of such fencing is not greater than four feet.

## SECTION 2: TYPE 2

### (a) Statement of Intent

- (1) The Type 2 site component may be utilized with non-residential developments that are adjacent to a residential district or other more restrictive district to help reduce potential negative impacts to the more restrictive district and to provide for an orderly transition of development intensity.
- (2) The Type 2 site component is intended to be utilized for residential development not meeting the intent of a Type 1 site component and not requiring the additional accessory structure or accessory dwelling privileges of the Type 3 site component.
- (3) This component is intended to be utilized with the majority of LO and LC use components except those that meet the intent of the Type 1 or Type 3 site component or with any use requiring drive-through service lanes.
- (4) This component is intended to be utilized with LO, LC, GC, HC, and HI use components when adjacent to residential districts and additional compatibility standards are warranted.
- (5) This component is generally not intended to be utilized with HC and HI use components except where such component is adjacent to, and not adequately buffered from, residential districts or other more restricted districts, and except as requested by the land owner.
- (6) Compliance with Type 1 standards shall also be deemed as compliance with this component.

### (b) Site Uses and Features (Some Site Uses Listed Also Require Appropriate Use Component)

- (1) General (All Development):
  - (i) Accessory buildings / structures are permitted providing that their total gross floor area square footage is not greater than ten percent (10%) of the gross floor

- area of the primary building / structure with the use utilizing such accessory building / structure(s), or one hundred twenty (120) square feet, whichever is greater (agricultural and recreational buildings / structures are exempt from this limit).
- (ii) Commercial outdoor animal boarding is not permitted.
  - (iii) Outdoor entertainment venues as a primary use involving the following are not permitted:
    - a. Substantial outdoor facilities;
    - b. Unshielded stadium lighting;
    - c. Noise generation; or
    - d. Outdoor amplified sound systems or similar conditions such as golf practice ranges open at night, sports stadiums not associated with an educational facility or public park, amusement parks, rodeo arenas, and similar facilities
    - e. Minor outdoor facilities such as tennis and basketball courts, track facilities, recreational equipment, play fields, and other similar facilities are permitted.
  - (iv) Accessory dwellings are permitted only where combined with non-residential, SFR, SFE and SFS components. Accessory dwellings shall contain a minimum of 400 square feet of living area and a maximum of 900 square feet of living area or 40% of the gross living area of the primary dwelling, whichever is greater. However, for lots greater than three acres in size, there is no limit to the size of the accessory dwelling.
- (2) Commercial and Industrial Development:
- (i) Drive-through service lanes and drive-in service are permitted. No drive-through service lane or service speaker shall be located within seventy-five (75) feet of a residential district unless such district is utilized for a non-residential use. Service speakers include speakers used to conduct business with people outdoors or in partially enclosed structures including, but not limited to, drive-through payment windows, drive-through restaurant ordering boards, service station pump islands and car washes.
  - (ii) Outdoor display is not permitted.
  - (iii) Outdoor storage and container storage are not permitted except for special vehicle storage as permitted by this ordinance.
  - (iv) Outdoor fuel sales are not permitted.
  - (v) Individual users shall not exceed 40,000 gross square feet of floor area in a single building. A single building may not exceed 60,000 square feet of floor area.
  - (vi) Overhead commercial service doors are not permitted.
  - (vii) Dumpster enclosures, grease traps, outdoor seating associated with restaurants, loading areas, or similar improvements are not permitted within seventy-five (75) feet of a residential district unless such district is utilized for a non-residential use.
- (3) Multi-Family Development:
- (i) At least thirty-five percent (35%) of the units are required to have at least one enclosed garage parking space and such garages are required to be leased, rented or sold with the applicable units.
  - (ii) Parking areas shall be no wider than two parking modules wide.

**SECTION 3: TYPE 3**

**(b) Site Uses and Features** (Some Site Uses Listed Also Require Appropriate Use Component)

- (2) Commercial and Industrial Development:

- (i) Drive-through service lanes and drive-in service are permitted as per Type 2 conditions.
- (ii) Outdoor display of merchandise is permitted when such display is of merchandise from a permanent business located in a permanent legal structure on site and such display is limited to not more than thirty percent (30%) of the gross floor area of the building which houses the business displaying such merchandise. Outdoor display of landscape plants, trees, shrubs, garden materials, etc. is not limited. Display is not permitted within required setbacks. Except for items that would normally be utilized and stored outside, outdoor display shall occur only during the business hours of the applicable business establishment.
- (iii) Outdoor storage and container storage are permitted as an accessory use as follows:
  - a. Such storage does not exceed twenty percent (20%) of the gross floor area of the primary building that houses the business storing such materials,
  - b. Such storage or container is located no closer to a street frontage than the primary building that houses the business storing such materials,
  - c. Such storage or container is screened from view from adjacent properties and any street in accordance with the Screening Requirements of Art. VI, Sec. 1.
  - d. Such storage or container is not permitted within required setbacks.
- (iv) The combination of outdoor display, outdoor storage and accessory buildings / structures shall not exceed forty percent (40%) of the gross floor area of the primary building / structure utilizing such. Outdoor storage or outdoor display that is covered or enclosed by an accessory building / structure shall be counted toward each individual limit for both outdoor storage / display and accessory buildings, but shall only be counted once toward the combined limit.
- (v) Outdoor commercial fueling and washing of vehicles meeting the following requirements is permitted:
  - a. Washing of vehicles (including vacuum facilities) is not located within one hundred fifty (150') feet of a residential district unless such district is utilized for non-residential purposes. The location is measured from the closest portion of the structure associated with the washing of vehicles to the residential district; and
  - b. Commercial fueling is not located within two hundred (200') feet of a residential district unless such district is utilized for non-residential purpose. The location is measured from the closest portion of the structure associated with the outdoor commercial fueling to the residential district; and
  - c. No more than two outdoor commercial fueling and washing of vehicles facilities shall be located within six hundred (600') feet of any intersection separately or in combination with a convenience store or other use.
  - d. Vehicle washing and fueling facility locations are reserved only by obtaining an approved building permit for such facility and keeping such permit active (see Article IX, Sec. 8 and 9).
  - e. Gasoline pumps shall be located no closer to the street than the primary structure.
- (vi) Overhead commercial service doors are permitted.
- (vii) Dumpster enclosures, grease traps, outdoor seating associated with restaurants, loading areas, or similar improvements are not permitted within seventy-five (75) feet of a residential district unless such district is utilized for a non-residential use.

## ARTICLE VI – SITE STANDARDS

### SECTION 1: LANDSCAPING, TREE PRESERVATION AND PROTECTION, AND SCREENING REQUIREMENTS

**(b) Landscape Requirements.**

(9) A minimum percentage of the total lot area of property on which development occurs shall be devoted to landscape development in accordance with the following schedule. Such percentages may include setback areas. However, all non-single-family and non-two-family setback areas are required to be landscaped in accordance with Article VI, Section 1 (b)(11) even if they exceed the following percentages:

- (i) Multifamily Dwellings, Tiny Home, Neighborhood Residential or Cluster Housing, 20%
- (ii) Office and Professional Uses, 15%
- (iii) Commercial Uses, 15%
- (iv) Industrial or manufacturing, 10%
- (v) Single-family-Dwellings and Two-Family dwellings, see Item (4) below
- (vi) Schools, churches, community centers and parks, 15%.

### SECTION 6: SETBACKS

**(a) Building / Structure and Site Improvements:**

BUILDING / STRUCTURE							
	Use Component	Architectural Component	Front	Side	Street Side	Rear	
<b>Standard Setback</b>	SFR		25'	7'	15'	15'	
	SFE, SFS, SFU, SFU/MH, TF		20'	5'	15'	15'	
	SFC, SFL		15' *	5' or 0' & 10'	15'	15'	
	<u>CH, TH, NR</u>		<u>15' *</u>	<u>5'</u>	<u>15'</u>	<u>15'</u>	
	SFT		10' *	0 or 10'	15'	15'	
	MF	Type A		20'	10'	20'	20'
		Type B		25'	10'	25'	20'
	LO, LC, GC, HC, HI	Type A, B		15'	10'	15'	10'
Type C			20'	10'	20'	10'	
GC, HC, HI	Type D		25'	15'	25'	15'	
<b>Special Setback Where Adjacent to SFR, SFE, SFS, SFU, SFC, SFL, SFU/MH, <u>CH, TH, NR, TF</u>**</b>	MF, LO, LC, GC, HC, HI	Type A	(NA)	20'	(N/A)	20'	
		Type B	(NA)	25'	(N/A)	25'	
	LO, LC, GC, HC, HI	Type C	(NA)	30'	(N/A)	30'	
	GC, HC, HI	Type D	(NA)	50'	(N/A)	50'	
<b>Garage Setback</b>	SFR, SFE, SFS, SFU, SFU/MH, SFC, SFL,		See Article VIII, Section 5 (i)				

	CH, NF, SFT, TF
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PARKING, AISLE, LOADING, CANOPIES, OUTDOOR DISPLAY						
	Use Component	Site Component	Front	Side	Street Side	Rear
Standard Setback	MF	All	20'	5'****	20'	5'****
	LO, LC, GC, HC, HI	Type 1, 2	15'	5'****	15'	5'****
		Type 3	20'	5'****	20'	5'****
		Type 4, 5	25'	5'****	25'	5'****
Special Setback Where Adjacent to SF, CH, or TF**	MF, LO, LC, GC, HC, HI	Type 1-3	(N/A)	15'	(N/A)	15'
		Type 4, 5	(N/A)	20'	(N/A)	20'

OUTDOOR STORAGE						
	Use Component	Site Component	Front	Side	Street Side	Rear
Standard Setback	LO, LC, GC, HC, HI	Type 3-4	***	5'****	***	0'
		Type 5	25'	0'****	25'	0'
Special Setback Where Adjacent to SF, CH, or TF**	LO, LC, GC, HC, HI	Type 3-5	(N/A)	25'	(N/A)	25'

\* In no case shall the garage be closer to the ROW than 18'

\*\* Unless such district is utilized for a non-residential use

\*\*\* No closer than the street facing wall of the primary structure that utilizes such storage.

\*\*\*\* Setback does not apply for parking, drive aisles, storage etc. that are intended to cross lot line.

\*\*\*\*\*

**SECTION 8: SINGLE-FAMILY, CLUSTER HOUSING, AND TWO-FAMILY LOTS ABUTTING OR ADJACENT TO TOLLWAY, HIGHWAY, ARTERIAL, OR COLLECTOR ROADWAY**

For single-family, cluster housing, and two-family lots contained within a subdivision receiving approval for a preliminary plat and final plat or a short form plat after the effective date of this ordinance and abutting or adjacent to a tollway, highway, arterial, or collector roadways as identified on the Leander Transportation Plan or based on roadway design, direct driveway access to any single-family, cluster housing, or two-family lot from such roadway shall be prohibited. The following are also applicable:

## SUBDIVISION ORDINANCE AMENDMENTS

### ARTICLE II. PROCEDURE

#### SECTION 21. CONCEPT PLAN

(a) **Purpose.** The purpose of the Concept Plan is to demonstrate conformance with the Master Plan, compatibility of the proposed development with this and other applicable City ordinances, and the coordination of improvements within and among individually platted parcels, sections, or phases of a development, prior to the consideration of a Preliminary Plat.

- (1) A Concept Plan shall be required for all subdivisions of land, except as otherwise provided for in this Ordinance for Short Form Final Plats.
- (2) The Concept Plan shall include all adjacent and contiguous land, owned or controlled by the developer or the person, firm or corporation that sold the tract being developed.

(3) It shall not be necessary to submit a Concept Plan on any land more than once, unless the concept substantially or materially changes, or approval of the precedent Concept Plan has expired, as defined in this Ordinance.

~~(3)~~(4) The Concept Plan shall demonstrate compliance with the Comprehensive Plan. The Comprehensive Plan calls for higher density residential and non-residential uses closer to the core of the established Centers. Areas located within one half mile of a Center shall also provide for higher density residential and transition to lower density residential as development moves away from the Centers.

(c) **Content.** The Concept Plan shall contain or have attached thereto:

- (1) Name, address and phone numbers of the developer, record owner, and authorized agents (engineer, surveyor, land planner, etc.)
- (2) Proposed name of the development; date revised and/or prepared; north indicator; scale.
- (3) Location map. Use of the latest USGS 7.5 minute quadrangle map is recommended.
- (4) A layout of the entire tract and its relationship to adjacent property, existing development and recorded plats.
- (5) The owner's name, deed or plat reference and property lines of property within two hundred (200) feet of the development boundaries, as determined by current tax rolls.
- (6) Topographic contours at ten (10) foot intervals, or less, unless otherwise approved by the City Engineer.
- (7) Proposed major categories of land use by acreage showing compatibility of land use with, or proposed variance from, the Master Plan. Demonstrate compliance with the location of land uses in context with the approved Comprehensive Plan and Future Land Use Map.
- (8) Proposed number of residential and non-residential lots, tracts or parcels together with the estimated
  - (i) number of LUEs required for each category of lots;
  - (ii) traffic volume to be generated by all proposed development and a Traffic Impact Analysis (or fee in lieu if approved by the City Engineer) if required by

the Roadway Adequacy ordinance (e.g.. projected average daily trips exceed 2,000).

## ARTICLE III. DESIGN STANDARDS

### SECTION 45. BLOCKS AND LOTS

Except as provided otherwise in this Section, the terms and provisions of the Composite Zoning Ordinance establishing the minimum lot area, width, setback line, side yard and rear yard requirements for each zoning or use category are incorporated herein by reference. Such regulations and standards shall be applied to property within the City limits based upon the zoning of the property and to property within the extraterritorial jurisdiction based on the closest applicable standard in the Composite Zoning Ordinance.

(b) **Lots.** All land area within the boundaries of the subdivision or re-subdivision except that area specifically dedicated as public right-of-way for any purpose shall be designated as a lot.

(3) The minimum lot size and development constraints for all lots approved to be served by on-site utilities shall be as follows.

(i) Lots approved to be served by on-site sewage treatment systems and to be connected to a community water supply shall be a minimum of one acre in size and conform to the County or LCRA regulations based on percolation tests and soils analysis.

(ii) Lots approved to be served by on-site sewage treatment systems and on-site private water wells shall be a minimum of two acres in size and conform to the County or LCRA regulations based on percolation tests and soils analysis.

~~(iii)~~ Lots approved to be served by on-site sewage treatment systems shall have steep topography, floodplain, or other natural features. This includes areas within the LCRA that is marked by steeper slopes and subject to strict regulations on wastewater discharge.

## SIGN ORDINANCE AMENDMENTS

### SECTION 3.08.006. DEFINITIONS AND INTERPRETATION.

Master Sign Plan means a plan establishing requirements for the size, location, and design of signs located within a planned development or a property located with a center or corridor as identified by the Comprehensive Plan.

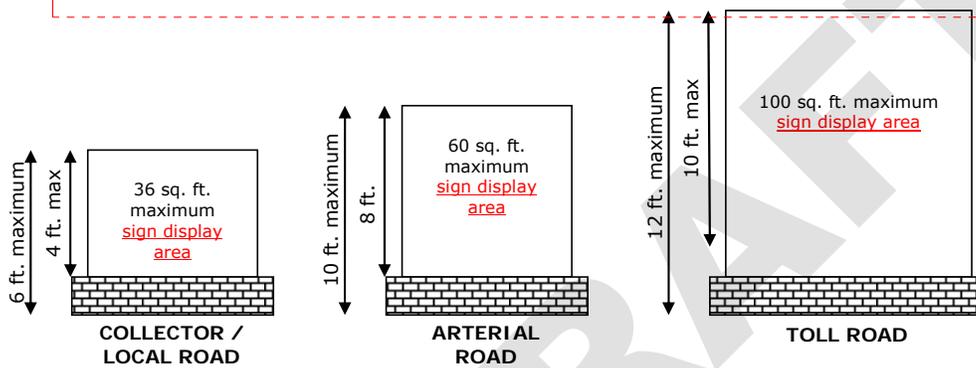
Primary Building Facade Frontage means...

### SECTION 3.08.012. PERMITTED SIGNS.

(1) Freestanding – Monument. Freestanding identification of a single business or plaza is permitted by a monument sign.

Site Criteria	Every lot is entitled to a monument sign. A spacing of one hundred fifty <u>(150')</u> feet shall be maintained between all monument signs. Additional monument signs shall be allowed for multi-tenant developments. These monuments may be permitted as long as they meet the minimum spacing requirements and that tenant only has signage on one monument per roadway.			
Sign Size by Road Type	<del>One per road frontage.</del> Freestanding signs shall be placed a minimum of five feet from any side property line. When more than one freestanding sign is allowed, a tenant's sign in the center shall be allowed on only one of the multi-tenant center signs.			
Sign Size By Road Type or Special Area		<b>Toll Road</b>	<b>Arterial Road</b>	<b>Collector / Local Road</b>
	Maximum square feet of sign display area	100	60	36
	Maximum Height	12'	10'	6'
Sign Structure Requirements - Bases, Caps, Proportion, etc.	No less than 75% of the width of the sign shall be in contact with the ground. For all signs between six and ten feet in height, a two-foot high stone, stucco or brick base shall be required			
Sign Display Area	<p>Items of information shall be surrounded by a margin of 10% of the smaller dimension of the sign display area of any panel contained in a freestanding sign. Freestanding cabinet signs shall have a surround around the cabinet <u>comprised of masonry compatible with the sign base</u>, and the cabinet shall be flush with or inset from sign surround.</p> <p>Street address shall be included on sign. The address shall not be calculated towards the sign area unless the street address is also the name of the center, business or development, or in such case that the street address exceeds six (6) square feet.</p> <p>For multi-tenant developments, the development name, if included as an integral part of the monument structure (i.e. not a separate panel), will not count towards the maximum sign face area as long as the area of the development name does not exceed 20% of the allowable maximum sign face area.</p> <p>For signs with multiple sign panels, the background color for all sign panels on the sign shall be consistent.</p>			

Sign Structure Materials	Monument signs shall be constructed of stone, brick or other masonry maintenance free material. The design and construction of both monument and signs shall be compatible with the architecture and style of the development on the project site. The base of the sign shall be protected by a landscape area to be maintained in a healthy condition.
Lighting	Internal illumination shall be permitted. External illumination and illumination by halation are permitted.
Electronic Messaging Center (EMC)	EMCs shall display text only for up to 1/3 <del>per</del> thirty-two (32) square feet of the sign display area, whichever is less.



Comment [RMG1]: I removed the arrows and dimensions for the width of the signs.

(6) Pylon Signs.

Site Criteria	Pylon signs are <del>only</del> permitted for <u>single-tenant or</u> multi-tenant development with a least 200 feet of frontage on a Toll Road.			
Sign Size by Road Type	One per site in lieu of another allowable freestanding sign unless otherwise approved as part of a Master Sign Plan. Freestanding signs shall be a minimum of five feet from any side property line.			
Sign Size By Road Type or Special Area		<b>Toll Road</b>	<b>Arterial Road</b>	<b>Collector / Local Road</b>
	Maximum square feet of sign display area for single tenant sign	70	0	0
	Maximum square feet of sign display area for multi-	150	0	0

	tenant sign			
	Maximum Height	25'	0	0
Sign Structure Requirements - Bases, Caps, Proportion, etc.	Two poles with a surround shall be required for the base of pylon signs. The spacing of the poles shall be no less than 75% of the width of the sign structure. The base of the pylon sign shall be constructed of stone, brick or other masonry maintenance free material. The design and construction of both the base and the signs shall be compatible with the architecture and style of the development on the project site.			
Sign Display Area	<p>Items of information shall be surrounded by a margin of ten percent of the smaller dimension of the sign display area of any panel contained in a freestanding sign. Cabinet signs shall be flush with or inset from sign surround.</p> <p>Street address shall be included on sign. The address shall not be calculated towards the sign area unless the street address is also the name of the center, business or development, or in such case that the street address exceeds six (6) square feet.</p> <p>For multi-tenant developments, the development name, if included as an integral part of the monument structure (i.e. not a separate panel), will not count towards the maximum sign face area as long as the area of the development name does not exceed 20% of the allowable maximum sign face area.</p> <p>For signs with multiple sign panels, the background color for all sign panels on the sign shall be consistent.</p>			
Sign Structure Materials	Stone, brick, cast stone, stucco, and/or a combination thereof. Cabinet signs shall be flush with or inset from sign surround.			
Lighting	External illumination, internal illumination, and illumination by halation shall be permitted. All internal lighting shall have concealed conduits. All external lighting sources shall be shielded from adjacent properties and rights-of-way.			
Electronic Messaging Center (EMC)	Not Permitted.			

(9) Wall.

Building Criteria	Building shall have a plane that can accommodate the placement of a sign.
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Number, Amount of Building Signage/Items of Information	Regardless of the length of primary building facade frontage, the owner/occupant is entitled to a sign of at least 20 square feet.					
Sign Size By Road Type per Occupant Primary Building Facade Frontage (Amount allowed shall be whichever measurement is less)		<b>Toll Road</b>	<b>Arterial Road</b>	<b>Collector / Local Road</b>	<b>Not Facing a Public ROW</b>	<b>Adjacent to Single Family or Two Family Zoned Property</b>
	Maximum square feet of sign display area	300	200	75	none	0
Square feet of sign display area per one linear foot of occupant primary building facade frontage	2.5	2.0	1.5	1.0	0	
Placement (on building)	A distance of no less than ten percent of the smallest dimension of the sign display area of a wall sign shall be provided around the entire sign display area and from any architectural features. The maximum height of the sign shall not project above the roof line, or top edge of parapet wall or mansard roof. Projection restricted – signs that project three (3) inches or more from the wall shall maintain a minimum clearance of nine (9) feet from the ground.					
Materials & Design	Internally lit tag lines and pan-faced signs shall be permitted as wall signs. All surfaces of a sign shall be finished. Signs shall be mounted to a building so that the attachment device shall not be visible or discernible. Wall signs shall be constructed of a rigid material. Wall signs exceeding sixteen (16) square feet of sign display area shall not be a single, flat surface.					
Lighting	Internal illumination shall be permitted <del>with the exception of in OS and DT special areas</del> . External illumination and illumination by halation are permitted.					

Electronic Messaging Center (EMC)	Not permitted.
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**SECTION 3.08.013. MASTER SIGN PLAN**

**A. Master Sign Plan.**

- (1) ~~The purpose of a Master Sign Plan is to allow an applicant, subject to the approval of the Building Official, to provide a consistent and architecturally compatible plan for multiple signs within a development. Master sign plans are intended for master planned commercial and residential developments. The plans are intended to encourage consistency and architectural compatibility among multiple signs within a development.~~ Master sign plans allow flexibility in the allocation of sign area between sign types as approved by the Building Official. In order to be approved, a master sign plan applicant must demonstrate that the plan achieves higher standards of sign design, architectural compatibility and overall project aesthetics.
- (2) Master sign plans only apply to non-residential projects.
- (3) ~~Master sign plans are required for projects located within a center as identified by the Comprehensive Plan. A consistent design standard is required and compliance with the standards included in the Composite Zoning Ordinance is required.~~
- (4) Application. The application shall include the following:
  - a. Proposed sign locations
  - b. Materials ~~proposed for all signs and sign structures~~
  - c. Type of illumination (~~external, internal, etc~~)
  - d. Design of free- standing sign structures
  - e. Size
  - f. Quantity
  - g. Uniform standards for nonbusiness signage, including directional and informational
  - h. ~~Color palette for all signs (letter colors, background, art, fonts, etc)~~
  - g-i. ~~Landscaping and/or ornamental structures~~
- (5) ~~Proposed sign types may include:~~
  - a. ~~Wall signs~~
  - b. ~~Monument signs~~
  - c. ~~Pylons~~
  - d. ~~Directional Signs~~
  - e. ~~Directory Signs~~
  - f. ~~Pedestrian Oriented Signs~~
  - h-g. ~~Other~~
- (5)(6) Compliance with Master Sign Plan. All applications for sign permits for signage within a multi-tenant occupancy shall comply with the master sign plan
- (6)(7) Signs listed as prohibited signs in Section 3.08.010 are not permitted.

**Access Management – Coordinate, plan, design, and implement land use and transportation strategies.**

**Recommendation** – Implement driveway standards

**Action** - update the zoning and subdivision ordinance to create standards for access management.

DRAFT

# COMPOSITE ZONING ORDINANCE AMENDMENTS

## ARTICLE VI – SITE STANDARDS

### SECTION 6: SETBACKS

Building / Structure and pavement setbacks will be applied based on street classification. Properties that are located on a Type A or B street will use the setback specified in Table (a), and properties that are located on C streets or other class streets will adhere to the building / structure and pavement setbacks outlined in Table (b). All properties will utilize the outdoor storage section under Table B.

A-Streets provide the highest level of pedestrian amenities and are intended to provide a walkable connection between commercial and residential uses. B-Streets are more automobile-oriented, but still provide pedestrian amenities and limit the amount of surface parking between buildings and the street. C-Streets are the most automobile oriented and provide for significant amounts of surface parking between buildings and the public right-of-way

Intent: The purpose of the setbacks in Table (a) are to limit the amount of parking in front of and between building facades and streets for non-residential uses.

(a) Maximum Building / Structure and Site Improvement Setbacks:

\*\* Minimum building setback.

<b>BUILDING / STRUCTURE</b>					
<u>Standard Setbacks</u>	<u>Street Type</u>	<u>Front maximum</u>	<u>Side minimum</u>	<u>Street Side maximum</u>	<u>Rear minimum</u>
	<u>Type A</u>	<u>15'</u>	<u>10'</u>	<u>15'</u>	<u>10'</u>
	<u>Type B</u>	<u>50'</u>	<u>10'</u>	<u>30'</u>	<u>10'</u>
<u>Special Setback Where Adjacent to SF or TF</u>	<u>Type A</u>	<u>(N/A)</u>	<u>15'</u>	<u>(N/A)</u>	<u>15'</u>
	<u>Type B</u>		<u>20'</u>		<u>20'</u>
<b>PARKING, AISLE, LOADING, CANOIES, OUTDOOR DISPLAY</b>					
<u>Standard Setbacks</u>	<u>Street Type</u>	<u>Front minimum</u>	<u>Side minimum</u>	<u>Street Side minimum</u>	<u>Rear minimum</u>
	<u>Type A</u>	<u>15'</u>	<u>5' ****</u>	<u>15'</u>	<u>5' ****</u>
	<u>Type B</u>	<u>5'</u>		<u>5'</u>	
<u>Special Setback Where Adjacent to SF or TF</u>	<u>Type A</u>	<u>(N/A)</u>	<u>15'</u>	<u>(N/A)</u>	<u>15'</u>
	<u>Type B</u>		<u>20'</u>		<u>20'</u>

\*\*\*\*Setback does not apply for parking, drive aisles, storage etc. that are intended to cross lot line.

(b) Building / Structure and Site Improvements:

BUILDING / STRUCTURE							
	Use Component	Architectural Component	Front	Side	Street Side	Rear	
Standard Setback	SFR		25'	7'	15'	15'	
	SFE, SFS, SFU, SFU/MH, TF		20'	5'	15'	15'	
	SFC, SFL		15' *	5' or 0' & 10'	15'	15'	
	SFT		10' *	0 or 10'	15'	15'	
	MF	Type A		20'	10'	20'	20'
		Type B		25'	10'	25'	20'
	LO, LC, GC, HC, HI	Type A, B		15'	10'	15'	10'
Type C			20'	10'	20'	10'	
GC, HC, HI	Type D		25'	15'	25'	15'	
Special Setback Where Adjacent to SFR, SFE, SFS, SFU, SFC, SFL, SFU/MH, TF**	MF, LO, LC, GC, HC, HI	Type A	(NA)	20'	(N/A)	20'	
		Type B	(NA)	25'	(N/A)	25'	
	LO, LC, GC, HC, HI	Type C	(NA)	30'	(N/A)	30'	
	GC, HC, HI	Type D	(NA)	50'	(N/A)	50'	
Garage Setback	SFR, SFE, SFS, SFU, SFU/MH, SFC, SFL, SFT, TF		See Article VIII, Section 5 (i)				
PARKING, AISLE, LOADING, CANOPIES, OUTDOOR DISPLAY							
	Use Component	Site Component	Front	Side	Street Side	Rear	
Standard Setback	MF	All	20'	5'****	20'	5'****	
	LO, LC, GC, HC, HI	Type 1, 2	15'	5'****	15'	5'****	
		Type 3	20'	5'****	20'	5'****	
		Type 4, 5	25'	5'****	25'	5'****	
Special Setback Where Adjacent to SF or TF**	MF, LO, LC, GC, HC, HI	Type 1-3	(N/A)	15'	(N/A)	15'	
		Type 4, 5	(N/A)	20'	(N/A)	20'	
OUTDOOR STORAGE							
	Use Component	Site Component	Front	Side	Street Side	Rear	
Standard Setback	LO, LC, GC, HC, HI	Type 3-4	***	5'****	***	0'	
		Type 5	25'	0'****	25'	0'	
Special Setback Where Adjacent to SF or TF**	LO, LC, GC, HC, HI	Type 3-5	(N/A)	25'	(N/A)	25'	

\* In no case shall the garage be closer to the ROW than 18'

\*\* Unless such district is utilized for a non-residential use

\*\*\* No closer than the street facing wall of the primary structure that utilizes such storage.

\*\*\*\* Setback does not apply for parking, drive aisles, storage etc. that are intended to cross lot line.

## ARTICLE V – SITE COMPONENTS

### SECTION 1: TYPE 1

**(b) Site Uses and Features:** (Some Site Uses Listed Also Require Appropriate Use Component)

(3) Commercial and Industrial Development:

(xii) Access Management- All nonresidential properties that have frontage onto an arterial or class street are required to provide a reverse frontage road shall have access to a lower class street. Access for these properties is prohibited from arterial or collector class streets. In the event physical conditions prohibit the use of a reverse frontage road, shared access may be permitted onto the arterial or collector class street per the following standards:

One point of access is permitted for properties with 600 contiguous feet of street frontage. Two points of access are permitted for properties with 1,000 contiguous feet of street frontage. Properties with greater than a 1,000 contiguous feet of frontage may be granted additional points of access at the discretion of the Director of Planning.

### SECTION 2: TYPE 2

**(b) Site Uses and Features:** (Some Site Uses Listed Also Require Appropriate Use Component)

(2) Commercial and Industrial Development:

(vii) Access Management is per Type 1 Site Component.

### SECTION 3: Type 3

**(b) Site Uses and Features:** (Some Site Uses Listed Also Require Appropriate Use Component)

(2) Commercial and Industrial Development:

(vii) Access Management is per Type 1 Site Component.

### SECTION 4: Type 4

**(b) Site Uses and Features:** (Some Site Uses Listed Also Require Appropriate Use Component)

(2) Commercial and Industrial Development:

(11) Access Management is per Type 1 Site Component.

### SECTION 5: Type 5

**(b) Site Standards**

(11) [Access Management is per Type 1 Site Component](#)

## ARTICLE IX - SITE DEVELOPMENT

### SECTION 3: PROCEDURES FOR APPROVAL OF SITE PLANS

#### (a) Site Plan Approval

The Planning Department shall not approve a site plan until all reviewing departments of the city have indicated that the proposed site plan, with all applicable attachments, complies with all applicable provisions of the code of ordinances of the city including the Composite Zoning Ordinance and the Subdivision Ordinance. [The applicant shall identify the whether the street the property is located on is of Type A, Type B, or Type C. A-Streets provide the highest level of pedestrian amenities and are intended to provide a walkable connection between commercial and residential uses. B-Streets are more automobile-oriented, but still provide pedestrian amenities and limit the amount of surface parking between buildings and the street. C-Streets are the most automobile oriented and provide for significant amounts of surface parking between buildings and the public right-of-way.](#) Park land and park improvements shall be required for all multi-family development in accordance with the standards of Sec. 61 of the Subdivision Ordinance. Development shall comply with regulations for Riparian Corridors set forth in Sec. 49 of the Subdivision Ordinance. The Planning Department shall approve a site plan if the proposed development complies with all applicable codes of the city. If the Planning Department finds that the site plan does not comply with all applicable codes of the city, the site plan shall be disapproved pending submittal of a site plan in compliance with the code of ordinances of the city. Approval of a site development permit authorizes the applicant to begin site development.

## SUBDIVISION ORDINANCE AMENDMENTS

### ARTICLE # II – PROCEDURE

#### SECTION 21. CONCEPT PLAN

##### (c) Content.

(18) [Identify Type A, B, and C streets that are located within Neighborhood, Community Center, Activity Center Nodes, and Employment Mixed Use Areas as identified by the City's adopted Comprehensive Plan.](#)

(19) [Any information required by the most recent City concept plan application/checklist.](#)

#### SECTION 22. PRELIMINARY PLAT

##### (3) Improvements.

(vii) [Identify all Type A, Type, or Type C streets within and adjacent to the subdivision boundary of the plat.](#)

(viii) Identify all shared use drives and necessary access easements as required by the Access Management standards listed in Article V – Site Components of the Composite Zoning Ordinance.

## **SECTION 24. FINAL PLAT**

(4) Improvements.

(viii) Identify all shared use drives and necessary access easements as required by the Access Management standards listed in Article V – Site Components of the Composite Zoning Ordinance.

## **SECTION 42. TRANSPORTATION IMPROVEMENTS**

**(b) Policy.**

(2) Additional street and trail design standards shall be as follows:

(iii) Streets and trails shall be extended to the boundary lines of the tract as necessary for eventual extension to the adjacent tract(s) to ensure that block lengths do not, in general, exceed the limits defined by this ordinance and to provide access to adjacent property. Residential subdivisions adjacent to and within Activity Centers, Community Centers, and Employment Mixed Use area are required to provide a street stub, pedestrian connection, and bicycle connection to adjacent properties with each subdivision. Those residential subdivisions within Neighborhood Centers are to provide either a pedestrian connection or bicycle path.

(vii) Provide Type A, B, and C Streets within Neighborhood Center, Community Center, Activity Centers, and Employment Mixed Use area as identified in the Comprehensive Plan. A-Streets provide the highest level of pedestrian amenities and are intended to provide a walkable connection between commercial and residential uses. B-Streets are more automobile-oriented, but still provide pedestrian amenities and limit the amount of surface parking between buildings and the street. C-Streets are the most automobile oriented and provide for significant amounts of surface parking between buildings and the public right-of-way.

Each quadrant of Activity Center, Community Center, Neighborhood Center, or Employment Mixed Use area as designated in the Comprehensive Plan shall include at least one A-Street which shall provide a pedestrian-oriented streetscape and shall be designed to provide connectivity through the entire quadrant and connect to adjacent residential neighborhoods. Commercial and multi-family projects within each quadrant of Town Center, Community Center and Neighborhood Center nodes shall have frontage on or connect to the A-Street.

## **SECTION 45. BLOCKS AND LOTS**

(3) Blocks along arterial streets and blocks containing or proposed to contain primarily commercial or industrial uses shall not exceed a perimeter length of ~~five-four~~ thousand ~~five hundred~~ (5,500,000) feet.

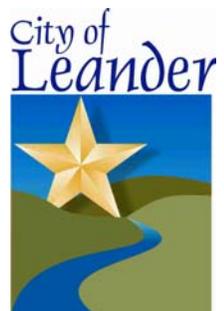
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## APPENDIX E TRANSPORTATION PLAN

### CITY OF LEANDER



REVISED 11/30/2016







# INTRODUCTION

## TRANSPORTATION PLAN INTRODUCTION

The purpose of the Transportation Plan is to encourage the development of a high quality multimodal transportation network that complements the overall community character and provides local accessibility and livability to its residents and visitors. In order to develop a successful multimodal transportation network, the City must incorporate elements of the Comprehensive Plan such as land use designations and appropriate functional classifications when planning its roads and highways.

The Transportation Plan was originally adopted in 2003 and has been updated over the years. This update includes changes to the roadway design standards and an update to the inventory and plan maps. The Transportation Plan includes vehicular, bicycle, pedestrian, and mass transportation components. The improvement and establishment of multiple modes of transportation is important to the comprehensive development of the City's transportation system.

This plan is divided into three sections:

### **Thoroughfare Plan and Map**

The Thoroughfare Plan establishes access management polices and intersection design preferences. The Thoroughfare Map identifies and describes the collector and arterial roadways. These roadway types provide for a grid of roadways that effectively connect destinations and encourage an efficient flow of traffic.

### **Pedestrian and Bicycle Facility Plan and Hike & Bike Map**

The Bicycle and Pedestrian Facility Plan identifies and describes the facilities designed for pedestrians and bicyclists. It also includes the Hike and Bike Map which identifies the location of the facilities that are proposed to be installed in order to provide for connectivity between different Centers and Corridors.

### **Transit Plan**

The Transit Plan will be further developed in the future to connect the rail station to Centers throughout the city.

## TRANSPORTATION POLICIES

- ◆ *Ensure that the roadway network is consistent with current and future growth.*
- ◆ *Land use and development should be taken into account when planning functional classifications and roadway design.*
- ◆ *During every new roadway project, ensure that the entire right-of-way is planned, designed, constructed, operated, and maintained to provide safe access for all users.*
- ◆ *Apply access management concepts along corridors to improve safety and mobility.*
- ◆ *Provide a safe and interconnected network of hike and bike trails throughout the community with connections to parks, neighborhoods, schools, the TOD and centers.*



# TRANSPORTATION PLAN

The Thoroughfare Plan and Map focus on motorized vehicles and roadways. The Map identifies a grid of collector and arterial roadways to provide a logical and practical roadway network.

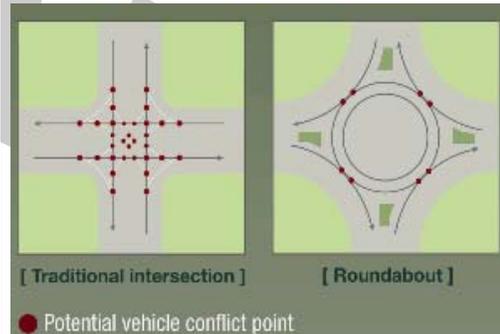
The City has adopted the City of Austin Transportation Criteria Manual (TCM). This document guides the design for transportation development in the City. The City of Leander Transportation Plan will \_\_\_\_\_ where the two documents conflict.

**Access Management:** Arterial and Collector Roads are intended to efficiently move traffic. Single-family and two-family residential lots are not permitted to have a driveway on a collector or arterial road. Site plans for commercial and other nonresidential uses should be designed to minimize the number of driveways while providing access to a site. Reverse frontage roads are encouraged along arterials. The Composite Zoning Ordinance restricts the frequency of driveways on to Arterials.

Driveways onto an arterial roadway with speeds greater than 45 MPH, Ronald Reagan Boulevard for example, shall include deceleration lanes.

**Intersection Design:** Intersections have a significant impact on the capacity and safety of the roadway network. Additional ROW is required at collector to arterial intersections and arterial to arterial intersections. The additional ROW will accommodate roundabouts or left and right turn lanes.

Roundabouts should be the first choice for collector to arterial intersections rather than a traditional traffic signal. Roundabouts have fewer fatal and injury accidents when compared to traffic signals and still maintain efficiency.



(Source Carmel, Indiana Police Department)

## Roundabout Resources:

- ◆ *Texas Roundabout Guidelines TxDOT Project 0-6414: Development of Guidelines for Implementation of Roundabouts in Texas*
- ◆ *FHWA's Roundabouts: An Informational Guide, Second Addition (AKA National Cooperative Highway Research Program 672)*
- ◆ *MythBusters: "Crossroads Conundrum"*



# TRANSPORTATION PLAN

The Thoroughfare Map is also used to determine the Roadway Adequacy Fees related to Boundary Street Improvements and to recommend the cross sections for roadways within the City. The chart provided on the Thoroughfare Map shall be used during the development process to calculate the minimum required right-of-way (ROW) dedication and the width of pavement for Roadway Adequacy Fees. In addition to the recommended right-of-way listed in the cross sections, additional right-of-way will be needed at certain intersections to accommodate turn lanes.

The right-of-way width and cross section elements shall ultimately be determined at the time of design. The roadway should be based on the adjacent land uses and recommendations of this plan.

The following roadway classifications are used in this plan and map.

- ◆ Arterial Roadways
- ◆ Collector Roadways
- ◆ Local Roadways
- ◆ Alleys



## ARTERIAL ROADWAYS

Arterials primarily provide for traffic movement, with a minor function of providing direct access to abutting property. Arterials typically serve as connections between major traffic generators and land use concentrations, and facilitate large volumes of through traffic traveling across a community. Because direct access to abutting property is a secondary function of arterial roadways, access should be carefully managed to avoid adverse impacts traffic flow on these facilities.

Cross section elements are defined in the table on the Thoroughfare Map.

### Roadway Features:

- ◆ Minimum Right-Of-Way: As defined in Thoroughfare Map Table
- ◆ A minimum of 20' of additional right-of-way shall be dedicated for a length of 200' prior to intersecting an arterial roadway to accommodate turn lanes.
- ◆ Minimum Number of Lanes: Typically 4 lanes with medians
- ◆ On-Street Parking: Not permitted
- ◆ Driveways: Most driveways are right in right out with median breaks and left turn lanes spaced in accordance with the Austin Transportation Criteria Manual and the Subdivision Ordinance (City Ordinances take precedence over TCM if there is a conflict.)
- ◆ Screening Wall: Required if residential homes side or back up to roadway – Masonry
- ◆ Pedestrian and Bicycle Facilities: Sidewalks and 5' bike lanes shall be provided on both sides
- ◆ Transit Facilities: Bus routes will be determined in the future



# TRANSPORTATION PLAN

Typical Arterial Cross Sections:

DRAFT

## Major Divided Arterial



# TRANSPORTATION PLAN

## COLLECTOR ROADWAYS

Collector roadways provide for a balance of traffic movement and property access functions. Traffic movement is often internal to localized areas, with collectors connecting residential neighborhoods, parks, churches, etc. with the arterial system and commercial areas. These roadways tend to carry a high volume of traffic over a much shorter distance while providing for land access. As compared to arterial roadways, collectors accommodate smaller traffic volumes over shorter distances.

The City has five typical collectors:

- ◆ Residential Collector 60'
- ◆ Residential Collector 64'
- ◆ Neighborhood Collector
- ◆ Primary Commercial Collector
- ◆ Secondary Commercial Collector





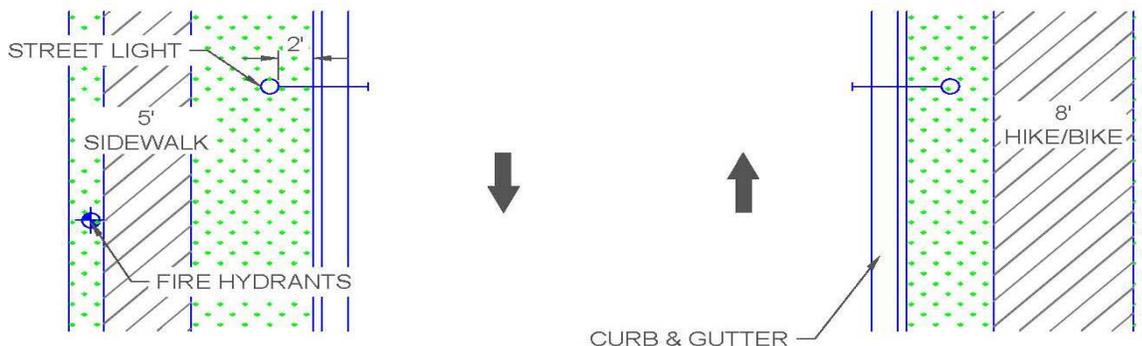
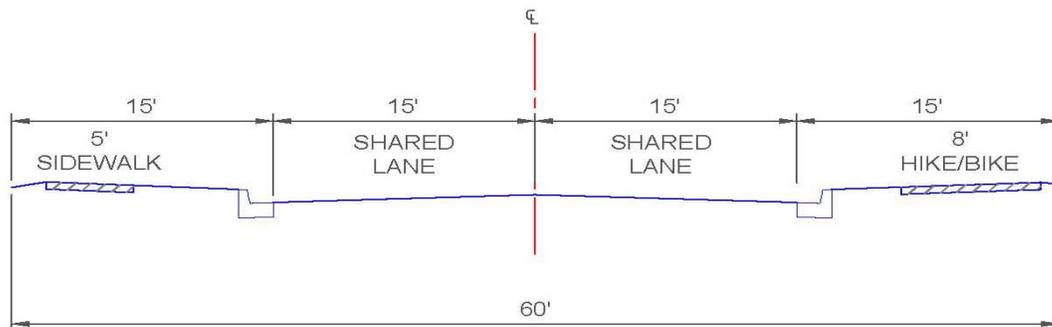
# TRANSPORTATION PLAN

## RESIDENTIAL COLLECTOR 60'

**General Description:** A residential collector is a low volume roadway that is typically internal to the subdivision or connecting the subdivision.

### Roadway Features:

- ◆ Minimum Right-Of-Way: 60'
- ◆ Maximum Number of Lanes: 2 lanes
- ◆ Minimum Lane Width: 15'
- ◆ Sidewalks: 8' Hike and Bike and a 5' Sidewalks
- ◆ On-Street Parking: Limited
- ◆ Driveways: Residential driveways are not permitted.
- ◆ Screening Wall: Required if residential homes side or back up to roadway – Masonry or concrete panel
- ◆ Bicycle Facilities: Shared lanes due to low volume of traffic
- ◆ Intersections: Pavement and ROW may widen to provide left turn lanes at intersections with other collectors or entrances to subdivisions, schools, amenity centers, etc. A minimum of 10' of additional right-of-way shall be dedicated for a length of 200' prior to intersecting an arterial roadway to accommodate left turn lanes. This assumes the right lane is a right turn/through lane.





# TRANSPORTATION PLAN

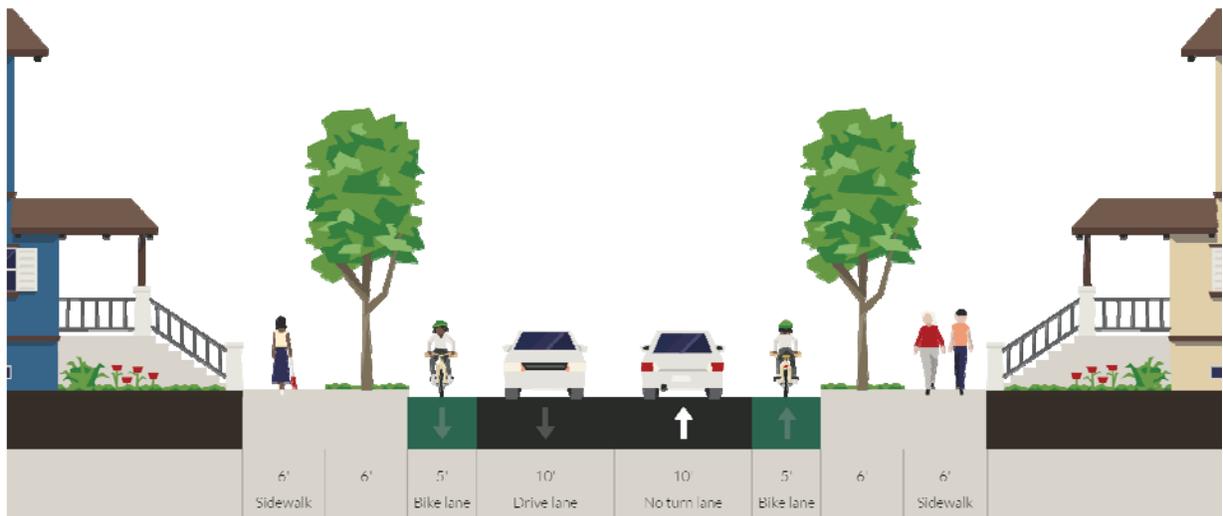
## RESIDENTIAL COLLECTOR 54'

**General Description:** A residential collector is a low volume roadway that is typically internal to the subdivision or connecting the subdivision.

### Roadway Features:

- ◆ Minimum Right-Of-Way: 54'
- ◆ Maximum Number of Lanes: 2 lanes
- ◆ Minimum Lane Width: 10'
- ◆ Sidewalks: 6' Sidewalks
- ◆ On-Street Parking: Not permitted
- ◆ Driveways: Residential driveways are not permitted.
- ◆ Screening Wall: Required if residential homes side or back up to roadway – Masonry or concrete panel
- ◆ Bicycle Facilities: 5' bike lanes
- ◆ Intersections: Pavement and ROW may widen to provide left turn lanes at intersections with other collectors or entrances to subdivisions, schools, amenity centers, etc. A minimum of 10' of additional right-of-way shall be dedicated for a length of 200' prior to intersecting an arterial roadway to accommodate left turn lanes. This assumes the right lane is a right turn/through lane.

## Residential Collector



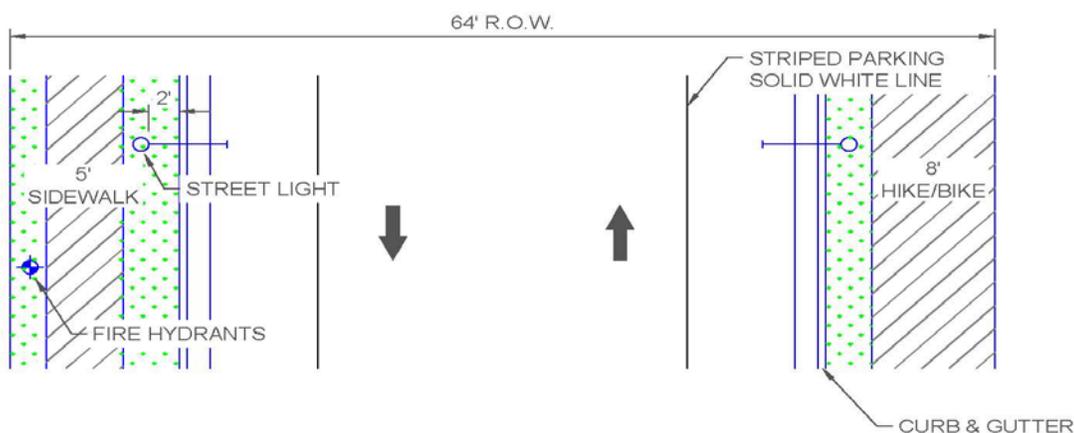
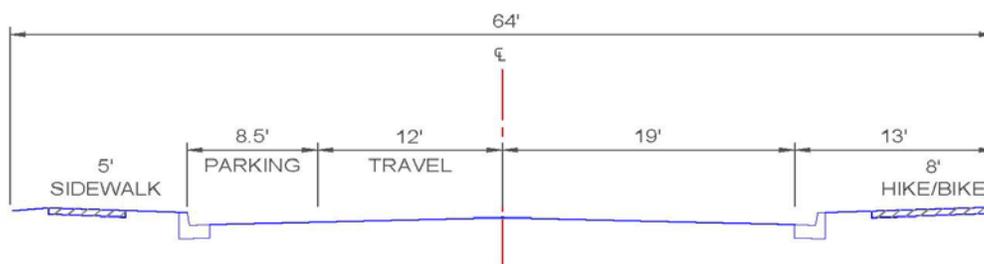
# TRANSPORTATION PLAN

## RESIDENTIAL COLLECTOR 64'

**General Description:** A residential collector is a low volume roadway that is typically internal to the subdivision or connecting the subdivision. This type of collector is for use when on street parking is necessary such as when adjacent to alley loaded homes, schools, or amenity centers fronting the collector.

### Roadway Features:

- ◆ Minimum Right-Of-Way: 64'
- ◆ Maximum Number of Lanes: 2 lanes
- ◆ Minimum Lane Width: see cross section
- ◆ On-Street Parking: Permitted. On street parking shall end to accommodate left turn lanes at intersections with other collectors or entrances to subdivisions, schools, amenity centers, etc.
- ◆ Screening Wall: Not typical as cross section is intended to be used when facilities are fronting the roadway
- ◆ Sidewalks: 8' Hike and Bike and a 5' Sidewalks
- ◆ Bicycle Facilities: Bicycle boxes may be provided adjacent to on street parking
- ◆ Intersections: Pavement and ROW may widen to provide left turn lanes at intersections with other collectors or entrances to subdivisions, schools, amenity centers, etc. A minimum of 10' of additional right-of-way shall be dedicated for a length of 200' prior to intersecting an arterial roadway to accommodate left turn lanes.



# TRANSPORTATION PLAN

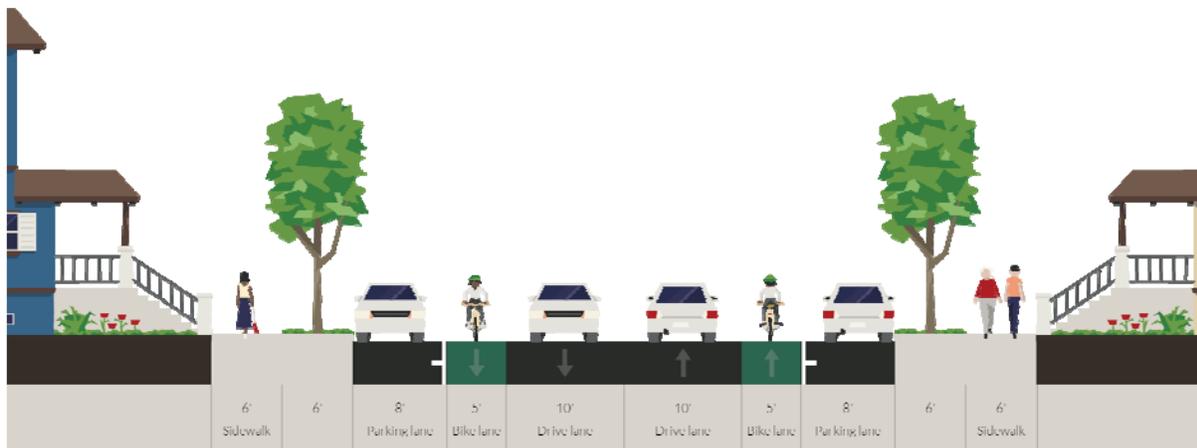
## RESIDENTIAL COLLECTOR 70'

**General Description:** A residential collector is a low volume roadway that is typically internal to the subdivision or connecting the subdivision. This type of collector is for use when on street parking is necessary such as when adjacent to alley loaded homes, schools, or amenity centers fronting the collector.

### Roadway Features:

- ◆ **Minimum Right-Of-Way:** 70'
- ◆ **Maximum Number of Lanes:** 2 lanes
- ◆ **Minimum Lane Width:** 10'
- ◆ **On-Street Parking:** Permitted. On street parking shall end to accommodate left turn lanes at intersections with other collectors or entrances to subdivisions, schools, amenity centers, etc.
- ◆ **Screening Wall:** Not typical as cross section is intended to be used when facilities are fronting the roadway
- ◆ **Sidewalks:** 6' Sidewalks
- ◆ **Bicycle Facilities:** 5' Bile lanes and bicycle boxes may be provided in limited areas in lieu of an on-street parking space
- ◆ **Intersections:** Pavement and ROW may widen to provide left turn lanes at intersections with other collectors or entrances to subdivisions, schools, amenity centers, etc. A minimum of 10' of additional right-of-way shall be dedicated for a length of 200' prior to intersecting an arterial roadway to accommodate left turn lanes.

## Res Collector w/ Parking





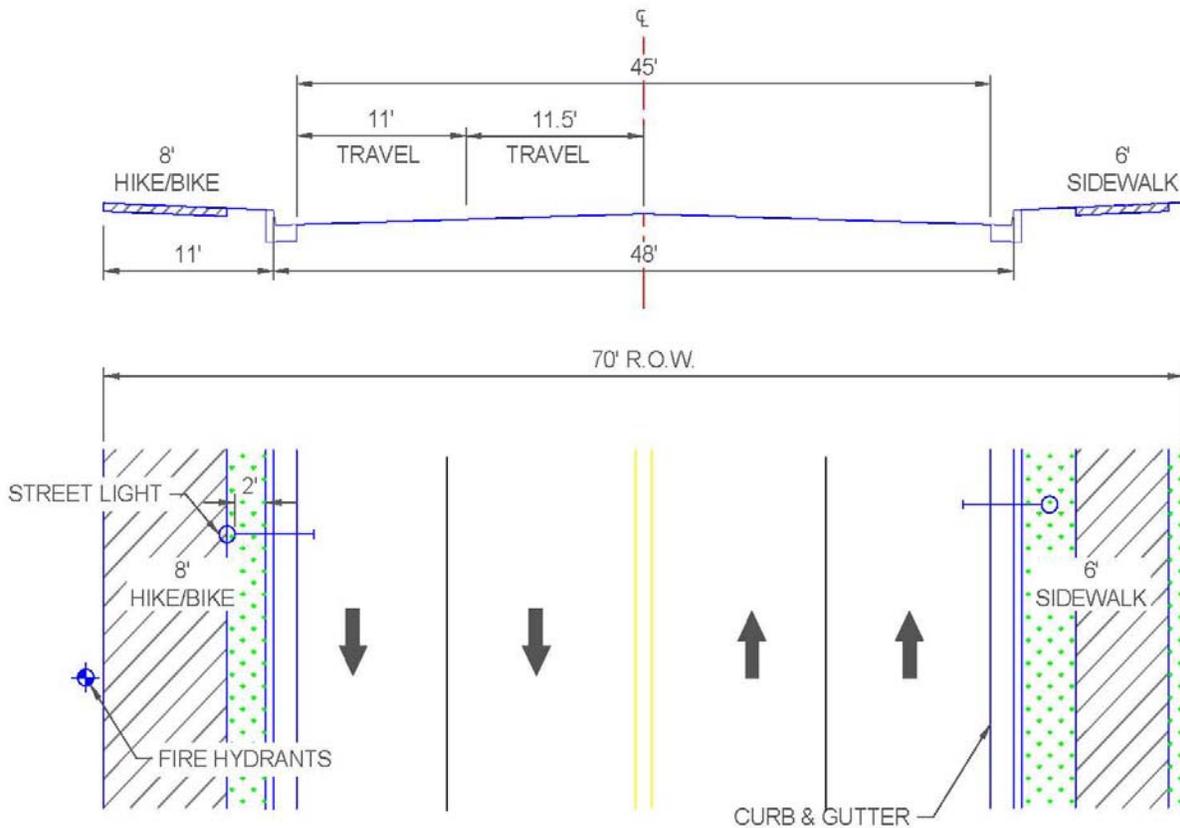
# TRANSPORTATION PLAN

## NEIGHBORHOOD COLLECTOR 70'

**General Description:** A neighborhood collector is a low volume roadway that typically connects multiple subdivisions and compatible uses such as churches, schools and local commercial. This is a four lane undivided roadway. The additional lanes accommodate cars turning from the through lanes while allowing traffic to continue.

### Roadway Features:

- ◆ Minimum Right-Of-Way: 70'
- ◆ Maximum Number of Lanes: 4 lanes
- ◆ Minimum Lane Width: see cross section
- ◆ On-Street Parking: Not permitted.
- ◆ Screening Wall: Required if residential homes side or back up to roadway – Masonry or concrete panel
- ◆ Sidewalks: 8' Hike and Bike and a 5' or 6' Sidewalks
- ◆ Bicycle Facilities: Hike and Bike trails shall be provided to accommodate bicyclists
- ◆ Intersections: Pavement and ROW may widen to provide left turn lanes at intersections with other collectors or entrances to subdivisions, schools, amenity centers, etc. A minimum of 10' of additional right-of-way shall be dedicated for a length of 200' prior to intersecting an arterial roadway to accommodate left turn lanes.





# TRANSPORTATION PLAN

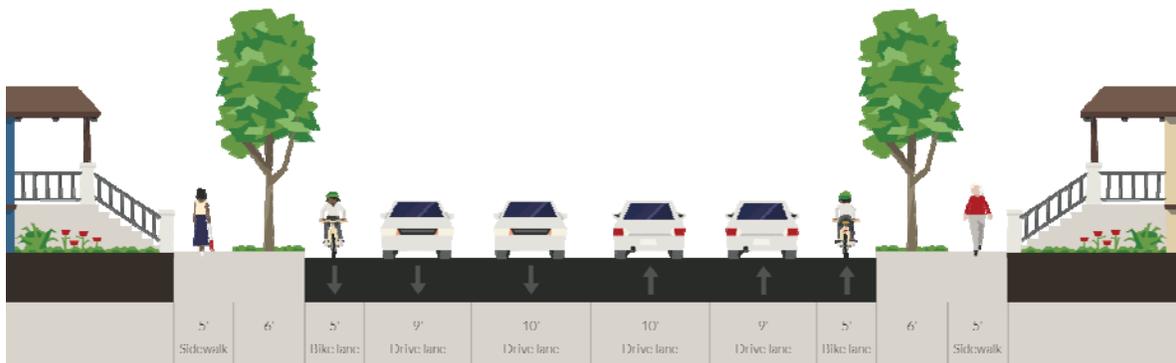
## NEIGHBORHOOD COLLECTOR 70'

**General Description:** A neighborhood collector is a low volume roadway that typically connects multiple subdivisions and compatible uses such as churches, schools and local commercial. This is a four lane undivided roadway. The additional lanes accommodate cars turning from the through lanes while allowing traffic to continue.

### Roadway Features:

- ◆ Minimum Right-Of-Way: 70'
- ◆ Maximum Number of Lanes: 4 lanes
- ◆ Minimum Lane Width: 9'
- ◆ On-Street Parking: Not permitted.
- ◆ Screening Wall: Required if residential homes side or back up to roadway – Masonry or concrete panel
- ◆ Sidewalks: 5' or 6' Sidewalks
- ◆ Bicycle Facilities: 5' bike lanes
- ◆ Intersections: Pavement and ROW may widen to provide left turn lanes at intersections with other collectors or entrances to subdivisions, schools, amenity centers, etc. A minimum of 10' of additional right-of-way shall be dedicated for a length of 200' prior to intersecting an arterial roadway to accommodate left turn lanes.

## Neighborhood Collector



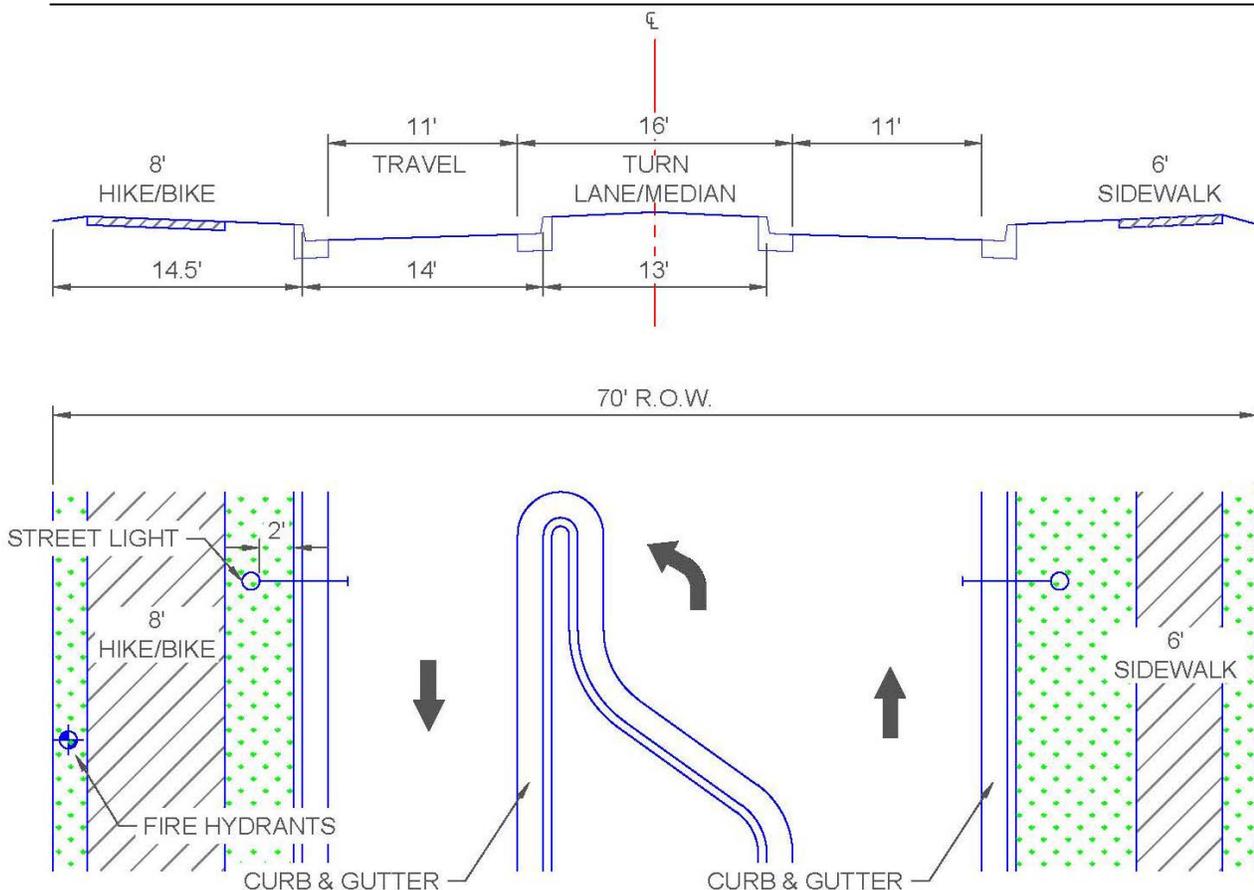
# TRANSPORTATION PLAN

## PRIMARY COMMERCIAL COLLECTOR

**General Description:** A primary commercial/industrial collector is a long commercial or industrial roadway. This type of collector provides connectivity between commercial/industrial uses as well as connecting neighborhoods with commercial uses.

### Roadway Features:

- ◆ Minimum Right-Of-Way: 70 feet
- ◆ Maximum Number of Lanes: 2 lanes and a left turn lane with a median
- ◆ Minimum Lane Width: 11 feet
- ◆ On-Street Parking: Not permitted
- ◆ Driveways: Most driveways are right in right out with median breaks and left turn lanes spaced in accordance with the Austin Transportation Criteria Manual
- ◆ Screening Wall: Required if residential homes side or back up to roadway – Masonry or concrete panel
- ◆ Sidewalks: 8' Hike and Bike and a 6' Sidewalk
- ◆ Bicycle Facilities: Hike and Bike trails shall be provided to accommodate bicyclists
- ◆ Intersections: A minimum of 10' of additional right-of-way shall be dedicated for a length of 200' prior to intersecting an arterial roadway to accommodate right turn lanes.



# TRANSPORTATION PLAN

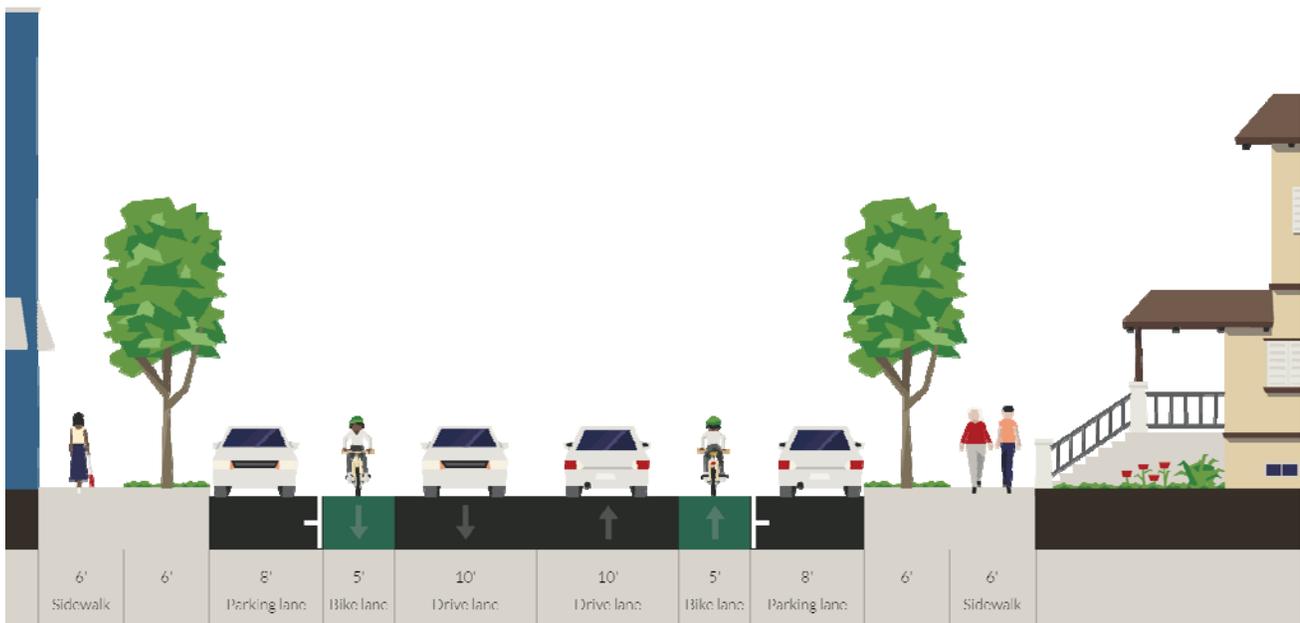
## MIXED USE COLLECTOR

**General Description:** A Mixed Use collector provides connectivity between commercial/ industrial uses as well as connecting neighborhoods with commercial uses.

### Roadway Features:

- ◆ Minimum Right-Of-Way: 70 feet
- ◆ Maximum Number of Lanes: 2 lanes
- ◆ Minimum Lane Width: 10 feet
- ◆ On-Street Parking: On-Street Parking: Permitted. On street parking shall end to accommodate left turn lanes at intersections with other collectors or entrances to subdivisions, schools, amenity centers, etc.
- ◆ Driveways: Permitted in accordance with the Austin Transportation Criteria Manual
- ◆ Screening Wall: Not typical but required if residential homes side or back up to roadway – Masonry or concrete panel
- ◆ Sidewalks: 6' Sidewalk
- ◆ Bicycle Facilities: 5' Bike lanes
- ◆ Intersections: A minimum of 10' of additional right-of-way shall be dedicated for a length of 200' prior to intersecting an arterial roadway to accommodate right turn lanes.

## Mixed Use Collector





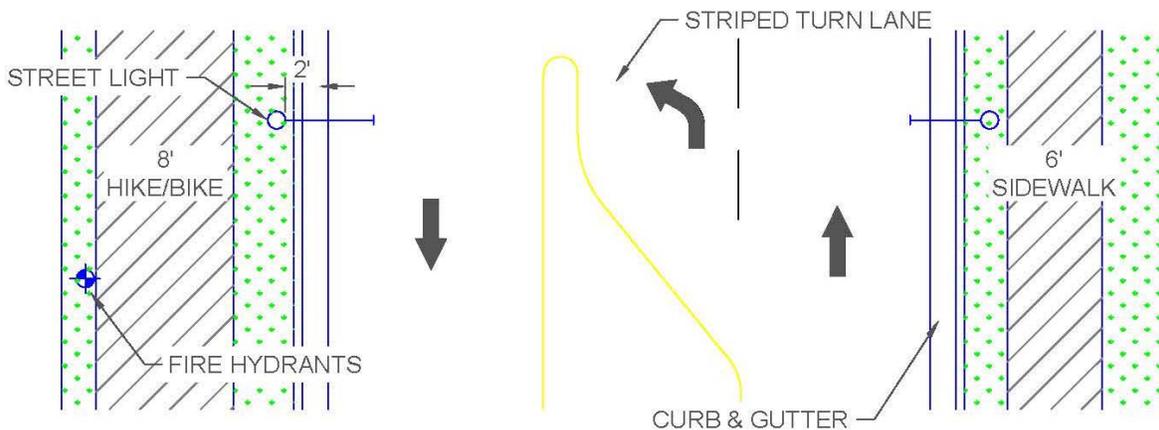
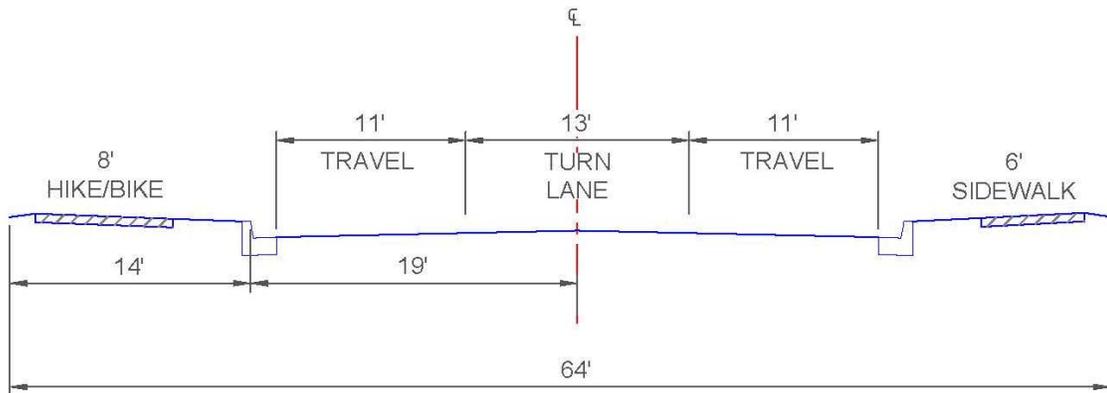
# TRANSPORTATION PLAN

## SECONDARY COMMERCIAL COLLECTOR

**General Description:** A secondary commercial/industrial collector is a short commercial or industrial roadway. This type of collector provides connectivity between commercial/industrial uses as well as connecting neighborhoods with commercial uses.

### Roadway Features:

- ◆ Minimum Right-Of-Way: 64'
- ◆ Maximum Number of Lanes: 2 lanes and a continuous two way left turn lane
- ◆ Minimum Lane Width: 11 feet
- ◆ On-Street Parking: Not permitted
- ◆ Screening Wall: Required if residential homes side or back up to roadway – Masonry or concrete panel
- ◆ Sidewalks: 8' Hike and Bike and a 6' Sidewalk
- ◆ Bicycle Facilities: Hike and Bike trails shall be provided to accommodate bicyclists
- ◆ Intersections: A minimum of 10' of additional right-of-way shall be dedicated for a length of 200' prior to intersecting an arterial roadway to accommodate right turn lanes.





# TRANSPORTATION PLAN

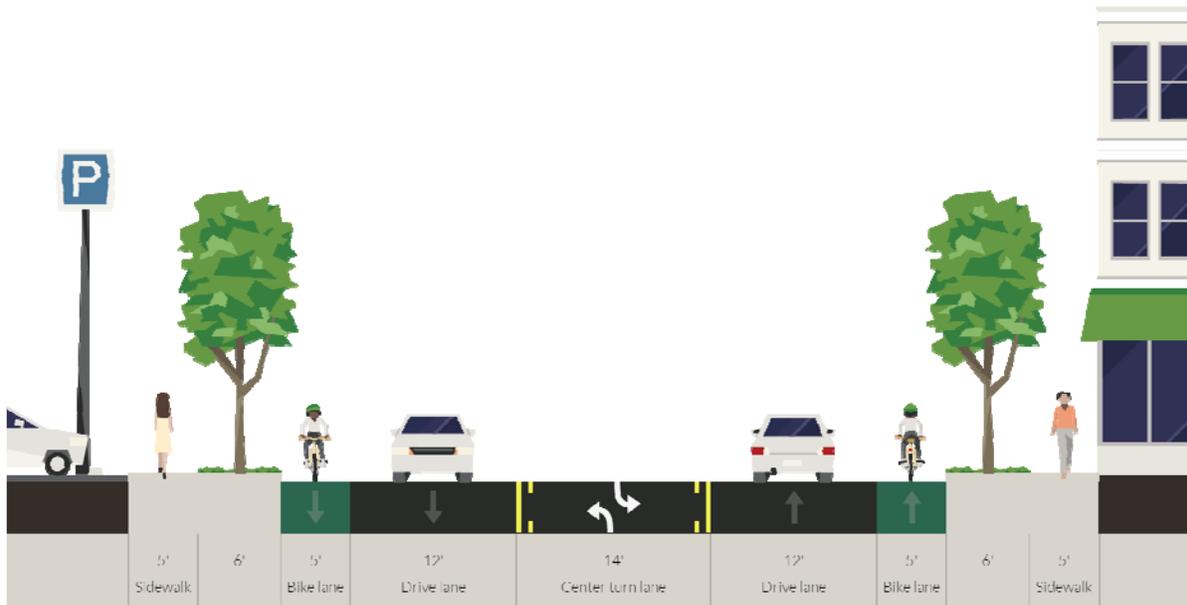
## Industrial Collector

**General Description:** An Industrial Collector provides connectivity between commercial and industrial uses. It is designed to accommodate frequent large-truck traffic.

### Roadway Features:

- ◆ Minimum Right-Of-Way: 70'
- ◆ Maximum Number of Lanes: 2 lanes and a continuous two way left turn lane
- ◆ Minimum Lane Width: 12'
- ◆ On-Street Parking: Not permitted
- ◆ Screening Wall: Required if residential homes side or back up to roadway – Masonry or concrete panel
- ◆ Sidewalks: 5' Sidewalk
- ◆ Bicycle Facilities: 5' bike lanes
- ◆ Intersections: A minimum of 10' of additional right-of-way shall be dedicated for a length of 200' prior to intersecting an arterial roadway to accommodate right turn lanes.

## Industrial Collector



# TRANSPORTATION PLAN

## LOCAL ROADWAYS

Local roadways provide access to adjoining residential land uses and distribute traffic to the surrounding roadway network. Most of the other roadways within the City of Leander that are not listed in any of the other descriptions are local roadways. The character of residential roadways can change from neighborhood to neighborhood. It should be noted that the roadways are not entirely for vehicular movement and are intended to accommodate pedestrians and bicyclists.

Not every typical local roadway cross section has been identified in the Plan. The cross sections will vary based on the character of the surrounding area and land uses. An example of a typical residential local road section is provided.

Developments subject to the *SmartCode* should follow standards set in Chapter 3 of the *SmartCode*.





# TRANSPORTATION PLAN

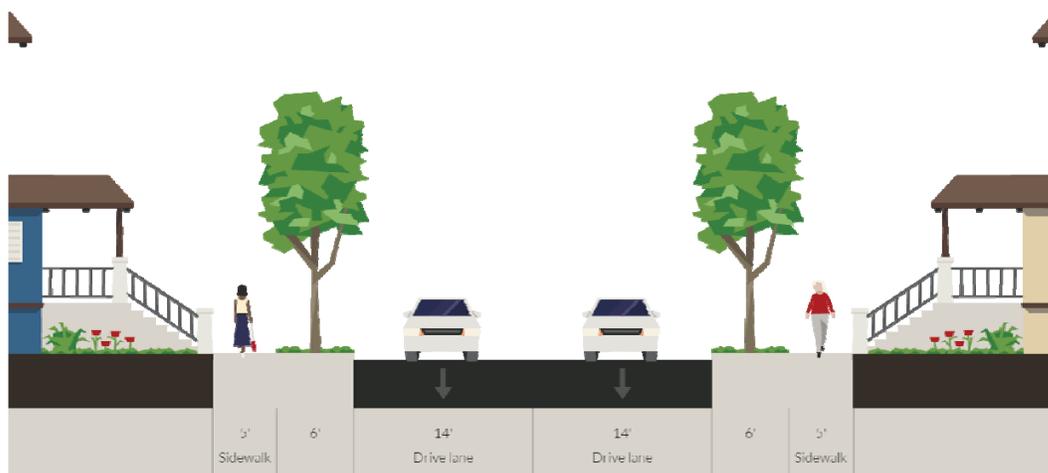
## RESIDENTIAL SUBURBAN LOCAL ROADWAY:

**General Description:** Residential local roadways provide access to single family residential homes. These roadways serve pedestrians, bicyclists, and vehicles.

### Residential Suburban Local Roadway Features:

- ◆ Minimum Right-Of-Way: 50'
- ◆ Maximum Number of Lanes: 2 lanes
- ◆ Minimum Lane Width: 14' including the gutter
- ◆ Sidewalks: 4' minimum and 5' recommended
- ◆ On-Street Parking: Permitted; through traffic can have a yield condition
- ◆ Bicycle Facilities: On street

## Residential Local



# TRANSPORTATION PLAN

## ALLEYS

Alleys run parallel to roadways and provide rear access to residential lots with rear entry garages or commercial buildings.

Old Town has several existing alleys. As per the *Smart Code*, new developments may utilize these existing alleys without improving or paving the alley.

Alleys must be designed to accommodate trash trucks. Attention should be given to curb returns for alley to alley intersections.

Two types of alleys are often proposed: Residential Alleys and Commercial Alleys.

### Residential Alley Features:

- ◆ Minimum Right-Of-Way: 20'
- ◆ Intended to provide access to rear entry garages
- ◆ Minimum Pavement Width: 14' of pavement and 3 feet of decomposed granite on each side
- ◆ Paving Materials: Public alleys are required to be concrete and private alleys may be concrete or asphalt. Asphalt alleys must include concrete ribbon curbs in the 14'.

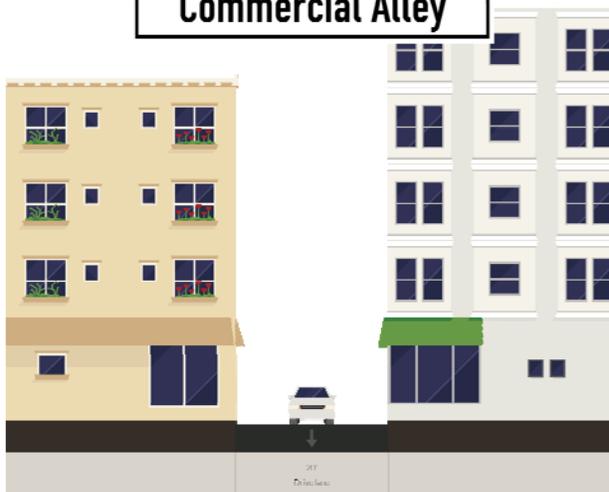
### Commercial Alley Features:

- ◆ Intended to provide access to commercial buildings in an urban area for fire protection, deliveries, unloading, etc.
- ◆ Minimum Right-Of-Way: 20'
- ◆ Minimum Pavement Width: 20' of pavement
- ◆ Paving Materials: Alleys are required to be concrete if directly adjacent to a building.

Residential Alley



Commercial Alley





# PEDESTRIAN & BICYCLE FACILITIES PLAN

The Pedestrian and Bicycle Facility Plan provides a plan for non-motorized transportation within the City. This plan will provide for alternatives to vehicular transportation and promote connectivity throughout the City. In addition, these facilities will provide opportunities for people that cannot or do not wish to drive vehicles to travel safely to their destination.

## **Pedestrians**

- ◆ All new roadways, except rural local roads, shall have bike lanes or hike and bike trails.
- ◆ Sidewalks on local residential roadways will be five feet wide.
- ◆ Sidewalks in commercial areas will be six feet wide.

## **Bicyclists**

- ◆ The Transportation Plan strives to provide alternatives to riding in the vehicle travel lane.
- ◆ Most arterials and collectors provide 5' bike lanes.
- ◆ Bicyclists share the road with cars in residential areas and on local roadways.
- ◆ Hike and Bike Trails provide a "off-street" network.

**Hike and Bike Trails** are along creeks. These facilities are typically constructed as the adjacent area is developed, though some are capital improvement projects through the Parks Department.

Some arterial roadways will have hike and bike trails instead of on-street bike lanes. The designs of these roads, Bagdad and Old 2243 for example, were completed prior to adopting the current policy of on-street bike lanes.

Most hike and bike trails in Leander will be shared by pedestrians and bicyclists. Sidewalks are intended for pedestrians. Major paths, such as those connecting the rail station to ACC, have parallel hike and bike trails which will reduce the traffic volume per trail. In the future shared facilities can be striped to divide pedestrians and bicyclist if the volume of traffic warrants it.

The Hike & Bike Map visually identifies the desired network of pedestrian and bicycle facilities.



## THOROUGHFARE MAP

The Thoroughfare Map on the next page provides roadway classifications for the roadways located with the City of Leander jurisdiction. The map also identifies the locations of the existing and proposed roadways throughout the City.

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