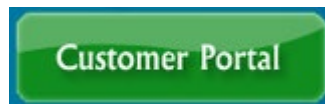


# Mobile Home Requirements

- A plot plan indicating where the mobile home will be located within the property lines and how many feet from each property line. Include: Dimensions of the home. A declaration page from the manufacturer.
- Proof of Homestead, if you are doing your own plumbing or electrical work (i.e. Tax Exemption from the county). The Homestead Affidavit form will need to be filled out and submitted at the time of applying for the permit.
- Detail from the manufacturer or contractor of how the home is to be tied down.
- Home must be tied down per City Code.
- Home must be under-pinned (skirting).
- A landing of at least 3' x 3' shall be at each door. Any landing that is more than 30" in height shall have guardrails and steps with handrails.
- Address on home must be visible from the street prior to the final inspection.
- Driveway must be asphalt or concrete. The width for a single driveway is a minimum of 8 feet to a maximum of 12 feet. Concrete depths must be 3 inches with wire mesh. Asphalt depths must be 2 ½ inches. Driveways proposed on state roads need to submit a copy of the approved permit from Texas Department of Transportation (TxDOT). If a waiver is needed on driveway installation please write a letter to the permits office, signed by the homeowners and the manufactured installer, noting the need for a 60 to 90 day extension for installation.

Apply



# Driveway – Required Documents when Submitting an Application

## Type I Driveway – Residential

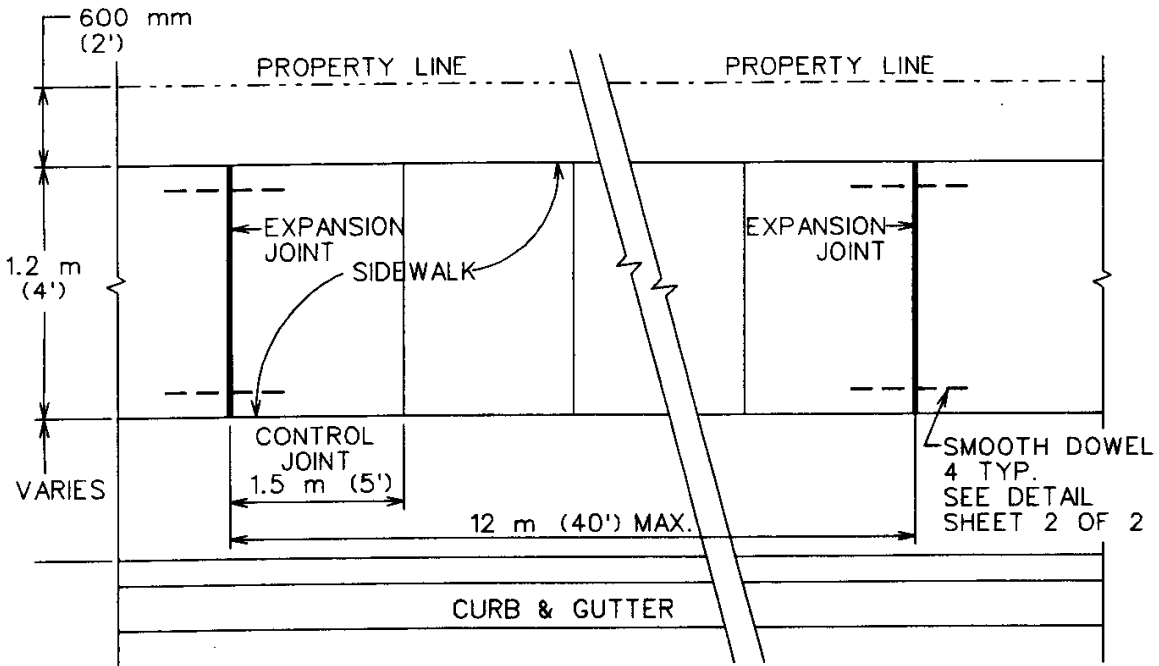
- Name of street driveway will access
- Location of proposed driveway
- Width of proposed driveway
- Distance from driveway to property lines
- Type of pavement surface to be used
- Curb radius on driveway entrance
- Location of streets/driveways within 100 ft.
- Existing or proposed sidewalks
- Type and size of proposed drainage culvert
- Location of existing inlets
- Location of utilities and/ or utility easements
- Location of trees > 8 caliper inches
- Any special site conditions
- Other information as needed to complete review

## Type II Driveway – Commercial

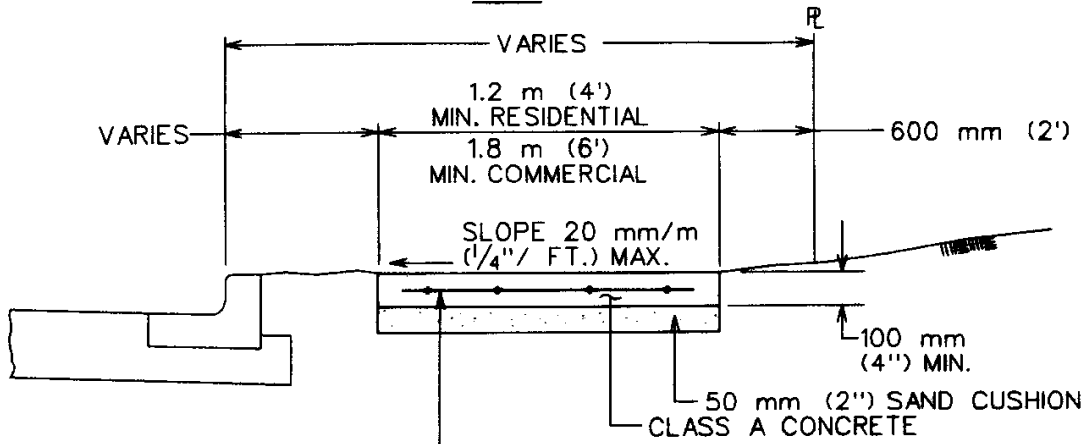
- Name of street driveway will access
- Location of proposed driveway
- Width of proposed driveway
- Throat length of proposed driveway
- Distance from driveway to property lines
- Type of pavement surface to be used
- Curb radius on driveway entrance
- Proposed slope of driveway
- Width of street to be accessed
- Location of street/driveways within 300 ft.
- Existing parking layout
- Existing or proposed sidewalks
- Type and size of proposed drainage culvert
- Location of existing inlets
- Location of utilities and/or utility easements
- Location of medians/median openings, if any
- Location of trees > 8 caliper inches
- Any special conditions
- Other information as needed to complete review

## Special Notes:

- Driveway design and construction must comply with the City of Austin standard specifications (Diagrams provided below) and all other applicable City of Leander ordinances. All traffic control shall be in accordance with the **Texas Manual on Uniform Traffic Control Devices**.
- It is the applicant's responsibility to contact all utility companies for the location of underground utilities. The applicant is responsible for any damage to existing utilities.



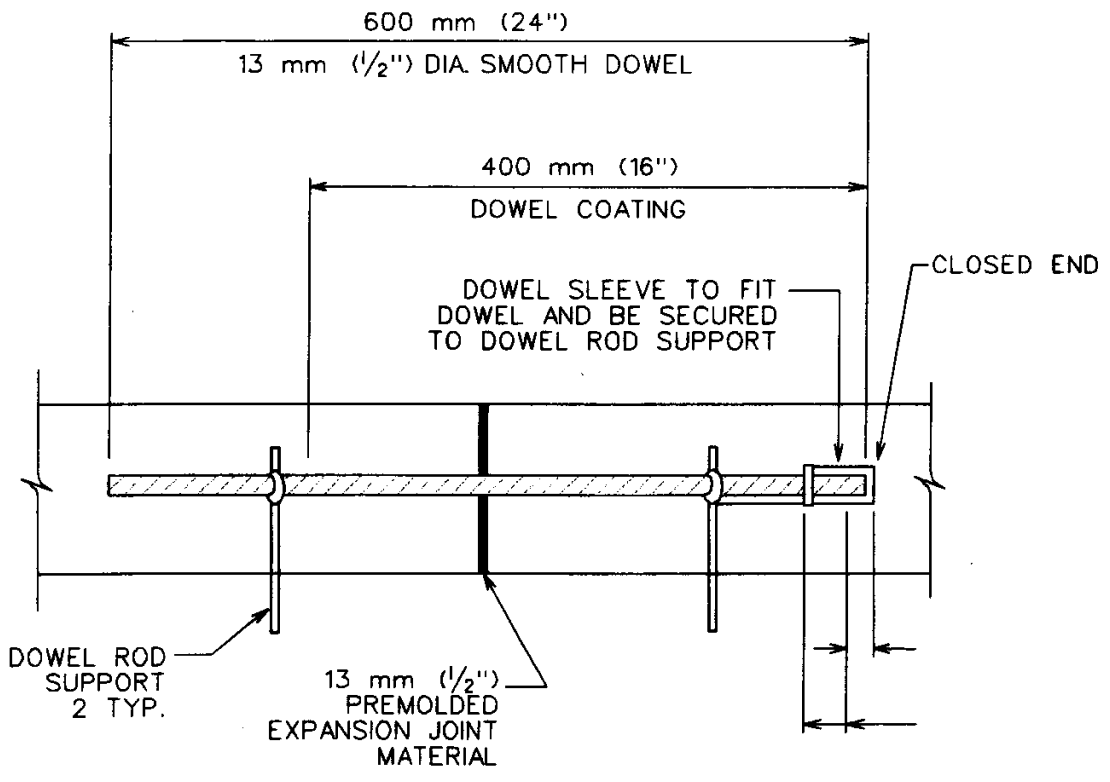
PLAN



SECTION

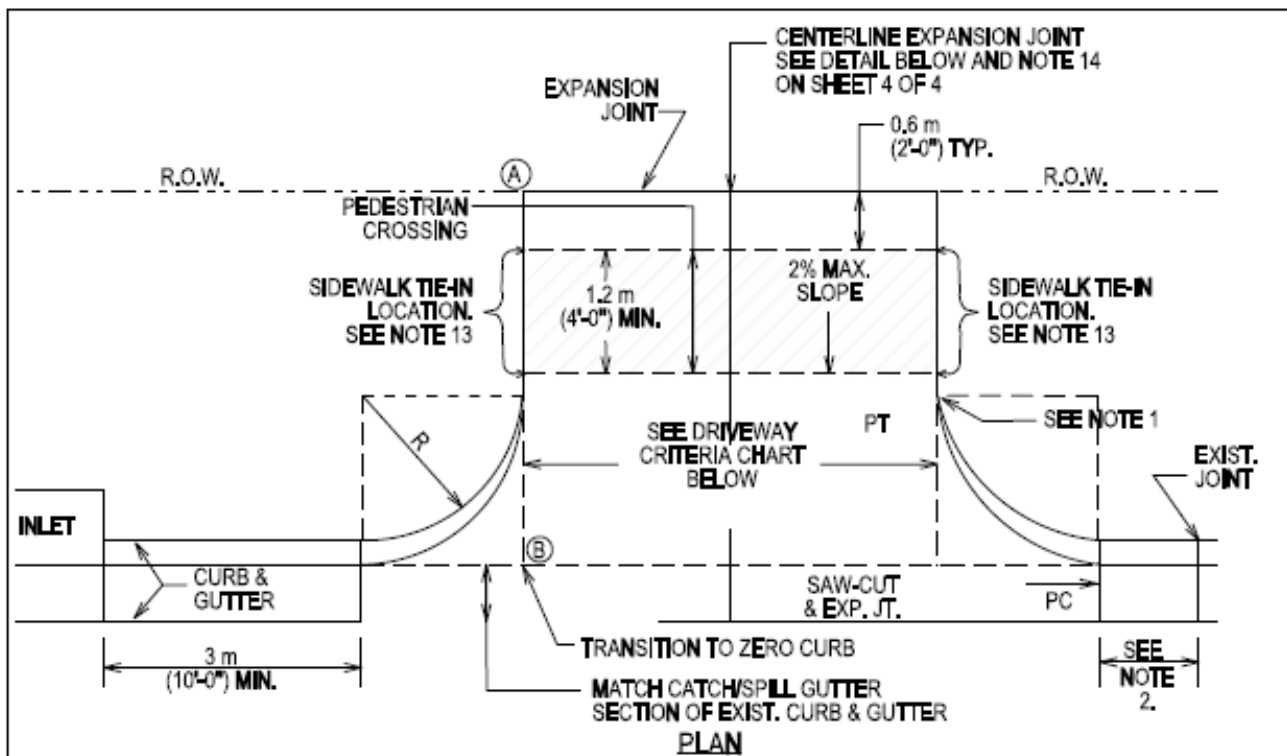
POLYPROPYLENE FIBRILLATED FIBERS, OR  
 150 mm X 150 mm X MW9 X MW9  
 (6" X 6" X W1.4 X W1.4) WELDED WIRE FABRIC  
 OR ONE LAYER 10M (#3) BARS PLACED NOT  
 MORE THAN 450 mm (18") C.C. BOTH  
 DIRECTIONS.  
 REINFORCEMENT SHALL ACCURATELY PLACED AT  
 SLAB MID-DEPTH AND HELD FIRMLY IN PLACE BY  
 MEANS OF BAR SUPPORTS OF ADEQUATE  
 STRENGTH AND NUMBER THAT WILL PREVENT  
 DISPLACEMENT AND KEEP THE STEEL AT ITS  
 PROPER POSITION DURING THE PLACEMENT OF  
 THE P.C. CONCRETE. IN NO INSTANCE SHALL THE  
 STEEL BE PLACED DIRECTLY ON THE SUBGRADE  
 OR SAND CUSHION LAYER.

CITY OF AUSTIN DEPARTMENT OF PUBLIC WORKS	SIDEWALK	
<i>Bill Gardner</i> 3/26/08 ADOPTED	THE ARCHITECT/ENGINEER ASSUMES RESPONSIBILITY FOR APPROPRIATE USE OF THIS STANDARD.	STANDARD NO. 432S-1 1 OF 3

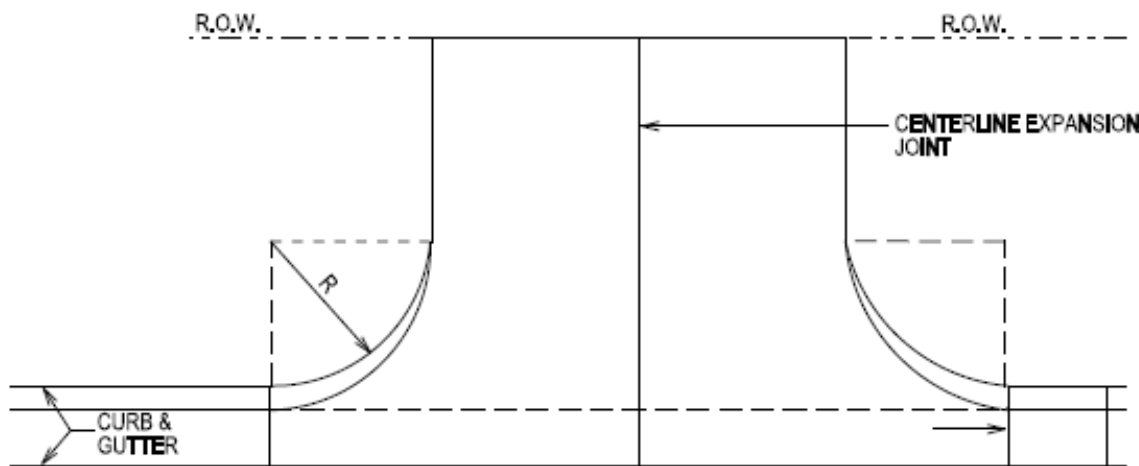


DOWEL DETAIL

CITY OF AUSTIN DEPARTMENT OF PUBLIC WORKS	SIDEWALK	
<i>Bill Gardner</i> 3/26/08 ADOPTED	THE ARCHITECT/ENGINEER ASSUMES RESPONSIBILITY FOR APPROPRIATE USE OF THIS STANDARD.	STANDARD NO. 432S-1 2 OF 3

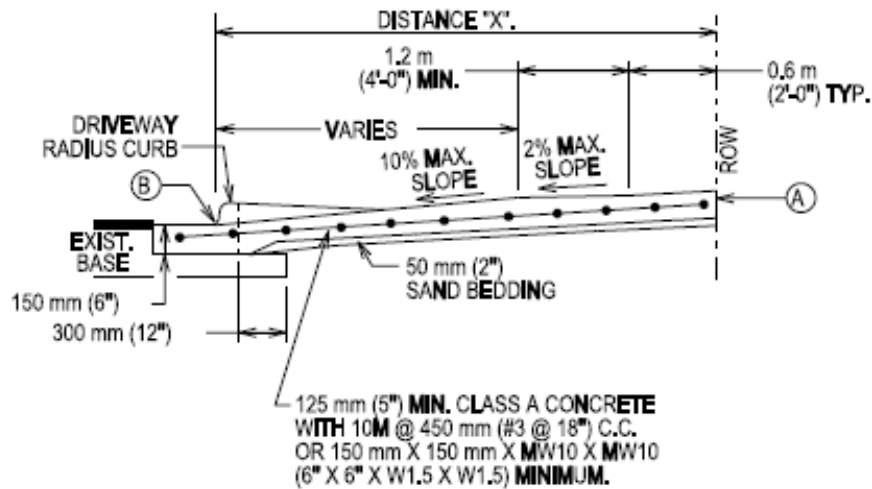


**NOTE:** ALL DRIVEWAYS SHALL BE SLOPED TOWARDS THE STREET FROM THE R.O.W. LINE. ELEVATION OF POINT A ABOVE POINT B IS, TYPICALLY A MINIMUM OF 150 mm (6") PLUS 20 mm/m (2" RISE/FOOT) OVER DISTANCE "X" IN METERS (FEET).



**PLAN**

<b>CITY OF AUSTIN</b> DEPARTMENT OF PUBLIC WORKS		<b>TYPE I DRIVEWAY</b> (1 & 2 FAMILY RESIDENTIAL USE ONLY)	
RECORD COPY SIGNED BY SAM ANGOORI	10/19/09 ADOPTED	THE ARCHITECT/ENGINEER ASSUMES RESPONSIBILITY FOR APPROPRIATE USE OF THIS STANDARD.	STANDARD NO. <b>433S-1</b> 1 OF 4



REINFORCEMENT SHALL ACCURATELY PLACED AT SLAB MID-DEPTH AND HELD FIRMLY IN PLACE BY MEANS OF BAR SUPPORTS OF ADEQUATE STRENGTH AND NUMBER THAT WILL PREVENT DISPLACEMENT AND KEEP THE STEEL AT ITS PROPER POSITION DURING THE PLACEMENT OF THE P.C. CONCRETE. IN NO INSTANCE SHALL THE STEEL BE PLACED DIRECTLY ON THE SUBGRADE OR SAND CUSHION LAYER.

**CROSS SECTION**

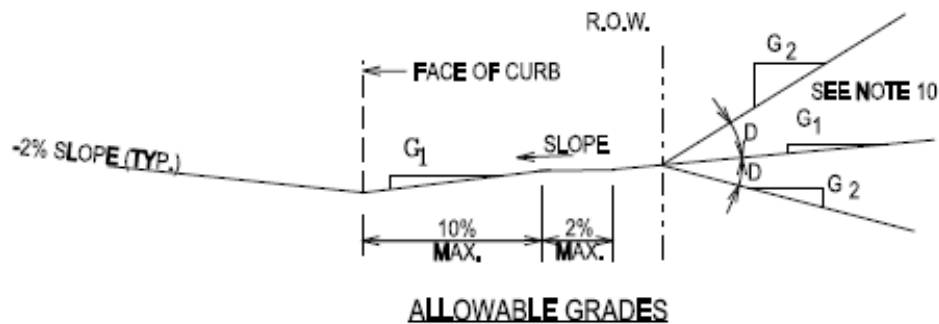
<p><b>CITY OF AUSTIN</b> DEPARTMENT OF PUBLIC WORKS</p>		<p><b>TYPE I DRIVEWAY</b> (1 &amp; 2 FAMILY RESIDENTIAL USE ONLY)</p>	
<p>RECORD COPY SIGNED BY SAM ANGOORI</p>	<p>10/19/09</p>	<p>THE ARCHITECT/ENGINEER ASSUMES RESPONSIBILITY FOR APPROPRIATE USE OF THIS STANDARD.</p>	<p>STANDARD NO. <b>433S-1</b> 2 OF 4</p>
<p>ADOPTED</p>			

DRIVEWAY CRITERIA	WIDTH METERS (FEET)		
	MIN.	*OPT.	MAX.
USE			
SIN. FAMILY	3,66 (12)	5,50 (18)	11,80 (25)
DUPLEX	4,56 (15)	5,50 (18)	11,80 (25)
TOWN HOME	4,56 (15)	5,50 (18)	11,80 (25)

\*OPTIMUM

USE	RADIUS DIMENSION METERS (FEET)		
	MIN.	*OPT.	MAX.
SINGLE FAMILY	1,5 (5)	1,5 (5)	3,0 (10)
DUPLEX	1,5 (5)	2,4 (8)	3,0 (10)
TOWN HOME	1,5 (5)	2,4 (8)	3,0 (10)

\*OPTIMUM



**CITY OF AUSTIN**  
DEPARTMENT OF PUBLIC WORKS

**TYPE I DRIVEWAY**  
(1 & 2 FAMILY RESIDENTIAL USE ONLY)

RECORD COPY SIGNED  
BY SAM ANGOORI

10/19/09

ADOPTED

THE ARCHITECT/ENGINEER ASSUMES  
RESPONSIBILITY FOR APPROPRIATE USE  
OF THIS STANDARD.

STANDARD NO.

**433S-1**

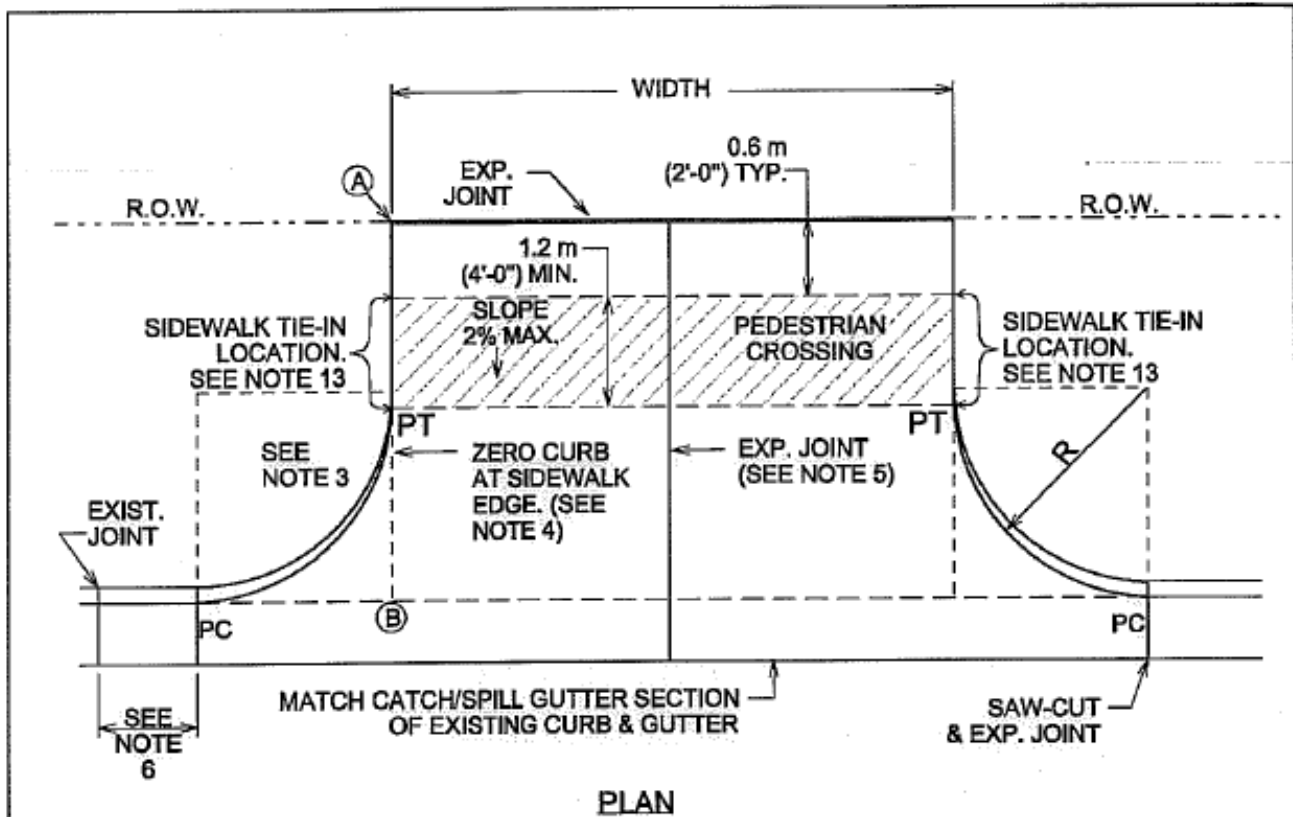
3 OF 4

**NOTES:**

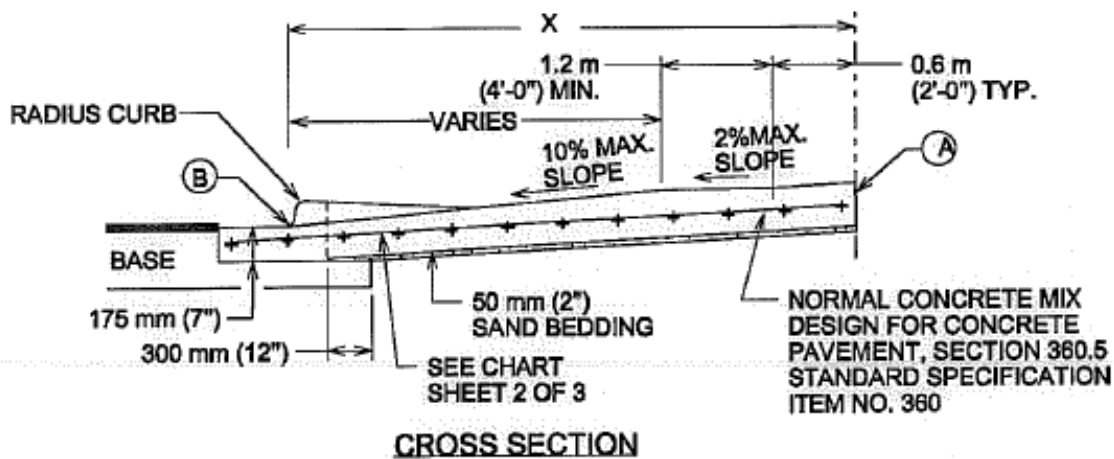
1. "ZERO" CURB AT PT OR SIDEWALK EDGE, WHICHEVER IS ENCOUNTERED FIRST. THE DRIVEWAY EDGE SHALL BE SMOOTHLY TRANSITIONED INTO THE SIDEWALK BEGINNING AT THE RADIUS PC LINE.
2. IF DIMENSION IS LESS THAN 1.5 METERS (5 FEET), REMOVE CURB AND GUTTER TO EXISTING JOINT AND POUR MONOLITHICALLY WITH THE DRIVEWAY.
3. IF THE BASE IS OVER EXCAVATED WHERE THE CURB AND GUTTER WAS REMOVED, BACKFILL WITH CONCRETE MONOLITHICALLY WITH THE DRIVEWAY.
4. ALL DRIVEWAYS MUST BE CONSTRUCTED WITHIN THE STREET FRONTAGE OF THE SUBJECT PROPERTY AS DETERMINED BY EXTENDING THE SIDE PROPERTY LINES TO THE CURB.
5. DRIVEWAYS SHALL NOT EXCEED 70% OF A LOTS' STREET FRONTAGE.
6. TYPE I DRIVEWAYS ARE TO BE LOCATED NO CLOSER TO THE CORNER OF INTERSECTING RIGHTS-OF-WAY THAN 60% OF PARCEL FRONTAGE OR 15 METERS (50 FEET); WHICHEVER IS LESS.
7. DRIVEWAYS SHALL NOT BE CONSTRUCTED WITHIN THE CURB RETURN OF A STREET INTERSECTION.
8. SINGLE FAMILY LOTS LIMITED TO ONE DRIVEWAY EXCEPT FOR APPROVED SEMICIRCULAR DRIVES.
9. WHEN TWO DRIVEWAYS ARE USED (ONE PER UNIT; TWO MAXIMUM) FOR DUPLEXES AND TOWN HOMES, SINGLE FAMILY STANDARDS SHALL APPLY.
10. WHILE THE PROPERTY OWNER REMAINS RESPONSIBLE FOR GRADE BREAKS WITHIN PRIVATE PROPERTY, THE FIRE DEPARTMENT SHOULD BE CONSULTED WHERE THE DRIVEWAY IS ESSENTIAL TO EMERGENCY VEHICLE ACCESS AND "G2" IS GREATER THAN 15%. "G1" PLUS "D" SHOULD NOT EXCEED 15%.
11. SEE TRANSPORTATION MANUAL SECTION 5 FOR OTHER DRIVEWAY REQUIREMENTS.
12. USE 12 mm ( $\frac{1}{2}$ ") ASPHALT BOARD, OR OTHER APPROVED MATERIAL, FOR CURB AND GUTTER EXPANSION JOINTS.
13. THE SIDEWALK, REGARDLESS OF ITS LOCATION WITH RESPECT TO THE CURB OR PROPERTY LINE, SHALL BE CONNECTED TO THE DRIVEWAY AT THESE LOCATIONS.
14. PLACE AN EXPANSION JOINT DOWN THE CENTER OF ALL DRIVEWAYS.
15. WATER METER BOXES AND WASTEWATER CLEAN OUTS ARE PROHIBITED FROM BEING LOCATED IN DRIVEWAY AREAS.

<b>CITY OF AUSTIN</b> DEPARTMENT OF PUBLIC WORKS		<b>TYPE I DRIVEWAY</b> (1 & 2 FAMILY RESIDENTIAL USE ONLY)	
<i>RECORD COPY SIGNED          BY SAM ANGOORI</i>		THE ARCHITECT/ENGINEER ASSUMES RESPONSIBILITY FOR APPROPRIATE USE OF THIS STANDARD.	
10/19/09 ADOPTED		STANDARD NO. <b>433S-1</b> 4 OF 4	





NOTE: ALL DRIVEWAYS SHALL BE SLOPED TOWARDS THE STREET FROM THE R.O.W. LINE. ELEVATION OF POINT (A) ABOVE POINT (B) IS, TYPICALLY A MINIMUM OF 150 mm (6") PLUS 20 mm/m (3/4" RISE/FOOT) OVER DISTANCE "X" IN METERS (FEET).



CITY OF AUSTIN  
DEPARTMENT OF PUBLIC WORKS

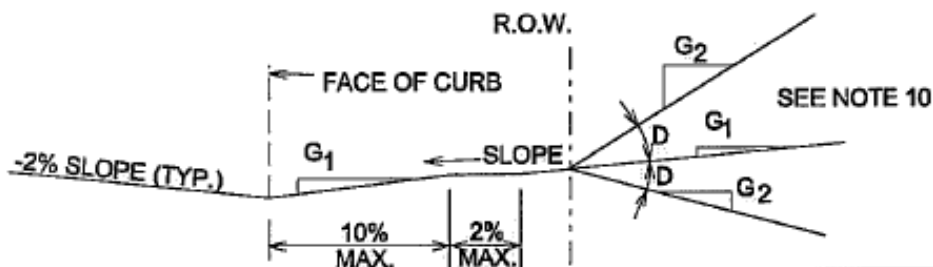
TYPE II DRIVEWAY

*[Signature]*  
2/24/16  
ADOPTED

THE ARCHITECT/ENGINEER ASSUMES RESPONSIBILITY FOR APPROPRIATE USE OF THIS STANDARD.

STANDARD NO.  
**433S-2**  
1 OF 2

USE	THICKNESS	REINFORCEMENT
DRIVEWAYS FOR PASSENGER VEHICLE PARKING LOTS	150 mm (6") MIN.	125 mm (5") MIN. CONCRETE WITH ONE LAYER OF 13M (#4) BARS PLACED ON CHAIRS AT MIDDEPTH OF SLAB AT NO MORE THAN 450 mm (18") O.C. BOTH DIRECTIONS
ALL OTHERS	175 mm (7") MIN.	125 mm (5") MIN. CONCRETE WITH ONE LAYER OF 13M (#4) BARS PLACED ON CHAIRS AT MIDDEPTH OF SLAB AT NO MORE THAN 450 mm (18") O.C. BOTH DIRECTIONS



### ALLOWABLE GRADES


DRIVEWAY VOLUME (ADT)	D=GRADE CHANGE	
	STD.	MAX
>1500	0%	3%
500-1500	3%	6%
< 500	6%	15%

#### NOTES:

- ALL TYPE II DRIVEWAYS SHALL HAVE RADIUS ENDS.
- DRIVEWAY WIDTHS AND RADIUS DIMENSIONS, ONE/TWO WAY TRAVEL REQUIREMENTS, AND GEOMETRIC LAY-OUT ARE HIGHLY VARIABLE. SUBJECT TO SITE SPECIFIC CONDITIONS AND REQUIREMENTS. SEE TRANSPORTATION CRITERIA MANUAL, SECTION 5 "DRIVEWAYS".
- THE DRIVEWAY EDGE SHALL BE SMOOTHLY TRANSITIONED INTO THE SIDEWALK TIE-IN LOCATION BEGINNING AT THE RADIUS PC LINE.
- "ZERO" CURB AT PT OR SIDEWALK EDGE, WHICHEVER IS ENCOUNTERED FIRST.
- PLACE AN EXPANSION JOINT DOWN THE CENTER OF DRIVEWAY ALL DRIVEWAYS.
- IF DIMENSION IS LESS THAN 1.5 METERS (5 FEET), REMOVE CURB AND GUTTER TO EXISTING JOINT AND POUR MONOLITHICALLY WITH DRIVEWAY.
- IF THE BASE IS OVER-EXCAVATED WHERE THE CURB AND GUTTER WERE REMOVED, BACKFILL WITH CONCRETE MONOLITHICALLY WITH THE DRIVEWAY.
- TYPE II DRIVEWAYS ARE TO BE LOCATED NO CLOSER TO THE CORNER OF INTERSECTING RIGHT OF WAY THAN 60% OF PARCEL FRONTAGE AT 30 METERS (100 FEET); WHICHEVER IS LESS.
- DRIVEWAY SHALL NOT BE CONSTRUCTED WITHIN THE CURB RETURN OF A STREET INTERSECTION.
- WHILE THE PROPERTY OWNER REMAINS RESPONSIBLE FOR GRADE BREAKS WITHIN PRIVATE PROPERTY, THE FIRE DEPARTMENT SHALL BE CONSULTED WHERE THE DRIVEWAY IS ESSENTIAL TO EMERGENCY VEHICLE ACCESS AND \*G2 IS GREATER THAN 15%.
- USE 12 MM (1/2") ASPHALT BOARD OR OTHER APPROVED MATERIAL FOR CURB AND GUTTER EXPANSION JOINTS. SIDEWALK, AT THE R.O.W. LINE AND AT MIDWIDTH, SEE NOTE 5.
- SEE TRANSPORTATION CRITERIA MANUAL, SECTION 5 FOR OTHER DRIVEWAY REQUIREMENTS.
- THE SIDEWALK, REGARDLESS OF ITS LOCATION WITH RESPECT TO THE CURB OR PROPERTY LINE, SHALL BE CONNECTED TO THE DRIVEWAY AT THESE LOCATIONS.
- WATER METER BOXES AND WASTEWATER CLEAN OUTS ARE PROHIBITED FROM BEING LOCATED IN DRIVEWAY AREAS.

CITY OF AUSTIN  
DEPARTMENT OF PUBLIC WORKS

### TYPE II DRIVEWAY

  
2/24/10  
ADOPTED

THE ARCHITECT/ENGINEER ASSUMES  
RESPONSIBILITY FOR APPROPRIATE USE  
OF THIS STANDARD.

STANDARD NO.  
**433S-2**  
2 OF 2