



**LEANDER CITY COUNCIL
and
PLANNING & ZONING COMMISSION
JOINT WORKSESSION
CITY OF LEANDER, TEXAS**

Pat Bryson Municipal Hall ~ 201 North Brushy Street ~ Leander, Texas

Thursday ~ September 15, 2016 at **5:30 p.m.**

**Mayor – Christopher Fielder
Place 1 – Andrea Navarrette
Place 2 – Michelle Stephenson
Place 3 – Shanan Shepherd**

**Place 4 – Ron Abruzzese (Mayor Pro Tem)
Place 5 – Jeff Seiler
Place 6 – Troy Hill**

**Place 1 – Chris Schwendenmann
Place 2 – Morgan Cotten
Place 3 – Jason Anderson
Place 4 – Sid Sokol (Chairman)**

**Place 5 – Richard Allen (Vice Chair)
Place 6 – Angela Means
Place 7 – Marshall Hines**

This meeting is open to the Public but does not allow for public participation

1. Open Work Session
2. Roll Call
3. Presentation and discussion of Comprehensive Plan implementation items for FY 2016-17
4. Presentation and discussion of proposed amendments to the Transportation Plan
5. Presentation from ACC on current status of the San Gabriel campus master plan
6. Adjournment

CERTIFICATION

This meeting will be conducted pursuant to the Texas Government Code Section 551.001 et seq. At any time during the meeting the Council reserves the right to adjourn into executive session on any of the above posted agenda items in accordance with the sections 551.071 [litigation and certain consultation with attorney], 551.072 [acquisition of interest in real property], 551.073 [contract for gift to city], 551.074 [certain personnel deliberations] or 551.076 [deployment/ implementation of security personnel or devices]. The City of Leander is committed to compliance with the American with Disabilities Act. Reasonable modifications and equal access to communications will be provided upon request. **Please call the City Secretary at 512/ 528-528-2743 for information.** Hearing impaired or speech disabled persons equipped with telecommunications devices for the deaf may call 512/ 528-2800. I certify that the above agenda for this Joint Work Session of City Council and the Charter Review Commission of the City of Leander, Texas, was posted on the bulletin board at City Hall, in Leander, Texas, on the 9th day of September, 2016 by 5:00 pm pursuant to Chapter 551 of the Texas Government Code.

Debbie Haile, TRMC, City Secretary



Executive Summary

September 15, 2016

Subject: Presentation and discussion of Comprehensive Plan implementation items for FY 2016-17.

Background: The City Council adopted an updated comprehensive plan in October of 2015. The new plan includes a number of implementation action items. The City Council has been reviewing the implementation actions at its retreats and providing direction to staff on the prioritization of the implementation actions.

At the July retreat, the City Council directed staff to proceed with the implementation actions on the attached document. Staff has also prepared a list of proposed ordinance amendments that are related to the FY 2016-17 implementation items.

Financial Consideration: None for this item.

Recommendation: This is a discussion item.

Attachments:

1. FY 2016-17 Comprehensive Plan Implementation Priorities
2. FY 2016-17 proposed ordinance amendments

Prepared by: Tom Yantis, Assistant City Manager

FY 2016-17 Comprehensive Plan Implementation Actions for Short- and Mid-Term Recommendations

Goal - Promote the TOD as an urban destination within a suburban community

Recommendation - Develop a highly visible public space within the TOD

Action - Extension of Metro Drive and planning of new recreation center and Brushy Creek trails

Recommendation - Recruit TOD Developers

Action - ongoing effort, current focus is with active developments in the TOD

Recommendation - Develop a P3 (Public/Private Partnership) Strategy for the TOD Area

Action - working with Capital Metro on an RFI (request for information) for a developer to redevelop the Capital Metro site

Recommendation - Define and participate in catalyst projects that will build momentum in the TOD and spur private investment

Action - work with Capital Metro on RFI for redevelopment, work with St. David's on new hospital site, work with ACC on joint planning for recreation center, complete Metro Drive extension and Brushy Creek trail extension, partner with other developers to facilitate development projects

Goal - Enhance Leander's public spaces to create and link destinations

Recommendation - Develop and implement streetscape designs for key corridors

Action - revise the zoning ordinance to create standard streetscape elements for major corridors and residential streets

Recommendation - Develop a wayfinding and signage program and create a sense of arrival into the city with gateways that reflect Leander's identity

Action - no action planned for FY 2016-17

Recommendation - Prepare a Public Space Master Plan

Action - no action planned for FY 2016-17

Recommendation - Develop a corridor beautification strategy

Action - update zoning ordinance to create standard streetscape elements for major corridors

Recommendation - Provide safe, sound and well-maintained neighborhoods

Action - conduct community clean ups

Recommendation - Enhance Leander's tree canopy

Action - continue to implement the tree preservation ordinance, develop a plan to utilize tree mitigation funds to plant new trees

Goal - Connect destinations

Recommendation - Implement a Complete Streets Policy to encourage safety, mobility and an active lifestyle in the City of Leander

Action - update the Transportation Plan to include Complete Street elements on all major thoroughfares and link with hike and bike trails

Recommendation - Improve bicycle and pedestrian amenities

Action - update the Transportation Plan, complete Brushy Creek trail construction

Recommendation - Work with Capital Metro and other interested parties to develop local area circulators for convenient access between neighborhoods, activity centers, and the regional transit network

Action - work with Capital Metro to begin first circulator bus service in 2017

Recommendation - Continue to plan and develop a functional transportation system that is coordinated with the Future Land Use Plan

Action - update the Transportation Plan, complete new transportation projects including Raider Way/Woodview, Metro Drive, and Brushy Creek trails

Goal - Create strong neighborhoods with a variety of housing options

Recommendation - Amend ordinances to accommodate a mixture of housing types

Action - Update the zoning and subdivision ordinance to provide for a mixture of housing types within neighborhoods. Revise the zoning ordinance to provide zoning categories for single-family detached condominiums and small scale multi-family products such as fourplexes, cottage courts, etc.

Goal - Foster civic pride

Recommendation - Conduct a branding study for the entire city

Action - no action planned for FY 2016-17

Recommendation - Consistently promote the City

Action - add a PIO

Recommendation - Capitalize on current and potential future visitor activities

Action - partner with the Chamber to enhance special events

Recommendation - Increase programming that brings the community together while simultaneously attracting visitors from outside Leander

Action - partner with the Chamber to enhance special events

Goal - Continue to expand infrastructure to serve Leander residents

Recommendation - Invest in utility planning that supports the land use pattern envisioned by the Future Land Use Map

Action - update the Water and Wastewater Master Plan, complete utility extension and oversizing projects

Goal - Continue to provide premium public safety services to Leander residents

Recommendation - Continue to provide excellent fire and emergency services in all areas of the community

Action - implement Police and Fire staffing recommendations for FY 2016-17, complete construction of Fire Station 4 and the relocation of Fire Station 1

Proposed Ordinance Amendments for Comprehensive Plan Implementation 2016-17

Composite Zoning Ordinance:

- Allow single-family detached condominiums by right without PUD
- Update commercial districts with additional compatibility standards when adjacent to single-family
- Update multi-family use and site components to provide for smaller scale, neighborhood multi-family options (include tiny homes)
- Develop corridor streetscape standards
- Develop residential streetscape standards
- Update single-family districts with maximum lot sizes and restrict SFR to areas unable to be served by sewer
- Update architectural requirements to require master architectural plans for centers (neighborhood, community, activity)
- Update setback standards within centers to limit parking in front of buildings on main streets within centers
- Update site design requirements to consolidate driveways within centers and along mixed-use and commercial corridors
- Update architectural standards for residential driveways to modify materials for three-car driveways, limit driveway width at street, etc.

Subdivision Ordinance:

- Update to limit lots that may be served by septic
- Update lot and block standards to ensure an interconnected street network within centers
- Update design standards to ensure the connection of centers to neighborhoods with streets and trails
- Coordinate with zoning ordinance to ensure the provision of a mix of residential housing types in neighborhoods (provide for specific lot types within 1/2 mile of centers and along residential collectors)

Transportation Plan:

- Update street cross-sections to include complete street elements

Transportation Criteria Manual:

- Revise street cross-sections
- Revise driveway spacing to provide better access management on corridors
- Revise maximum driveway width for residential driveways

Sign Ordinance:

- Require master sign plan within centers - require consistent design standard on corridors



Executive Summary

September 15, 2016

Subject Presentation and discussion of proposed amendments to the Transportation Plan.

Background: One of the implementation actions from the updated comprehensive plan is to update the transportation plan to incorporate complete streets components. Staff began working on a draft update to the transportation plan earlier this year and held a work session with P&Z to discuss the draft. P&Z provided feedback to staff at their July 28th work session and this item will allow Council and P&Z to discuss how to proceed with the transportation plan update.

Financial Consideration: None for this item.

Recommendation: This is a discussion item.

Attachments:

1. P&Z minutes from the July 28, 2016 work session
2. Transportation Plan draft
3. Thoroughfare map
4. Thoroughfare table
5. Hike & Bike map

Prepared by: Tom Yantis, Assistant City Manager

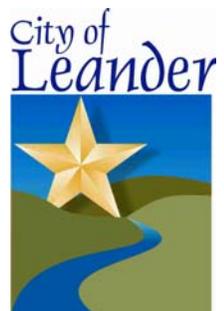
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APPENDIX E TRANSPORTATION PLAN

CITY OF LEANDER



REVISED 07/21/2016





INTRODUCTION

TRANSPORTATION PLAN INTRODUCTION

The purpose of the Transportation Plan is to encourage the development of a high quality multimodal transportation network that complements the overall community character and provides local accessibility and livability to its residents and visitors. In order to develop a successful multimodal transportation network, the City must incorporate elements of the Comprehensive Plan such as land use designations and appropriate functional classifications when planning its roads and highways.

The Transportation Plan was originally adopted in 2003 and has been updated over the years. This update includes changes to the roadway design standards and an update to the inventory and plan maps. The Transportation Plan includes vehicular, bicycle, pedestrian, and mass transportation components. The improvement and establishment of multiple modes of transportation is important to the comprehensive development of the City's transportation system.

This plan is divided into three sections:

Thoroughfare Map

The Thoroughfare Map identifies and describes the collector and arterial roadways. These roadway types provide for a grid of roadways that effectively connect destinations and encourage an efficient flow of traffic.

Pedestrian and Bicycle Facility Plan and Hike & Bike Map

The Bicycle and Pedestrian Facility Plan identifies and describes the facilities designed for pedestrians and bicyclists. It also includes the Hike and Bike Map which identifies the location of the facilities that are proposed to be installed in order to provide for connectivity between different Centers and Corridors.

Transit Plan

The Transit Plan will be further developed in the future to connect the rail station to Centers throughout the city. This system is still in the early planning stages.

TRANSPORTATION POLICIES

- ◆ *Ensure that the roadway network is consistent with current and future growth.*
- ◆ *Land use and development should be taken into account when planning functional classifications and roadway design.*
- ◆ *During every new roadway project, ensure that the entire right-of-way is planned, designed, constructed, operated, and maintained to provide safe access for all users.*
- ◆ *Apply access management concepts along corridors to improve safety and mobility.*
- ◆ *Provide a safe and interconnected network of hike and bike trails throughout the community with connections to parks, neighborhoods, schools, the TOD and centers.*



TRANSPORTATION PLAN

The Thoroughfare Map focuses on motorized vehicles and roadways. It identifies a grid of collector and arterial roadways to provide a logical and practical roadway network. The Thoroughfare Map is also used to determine the Roadway Adequacy Fees related to Boundary Street Improvements and to recommend the cross sections for roadways within the City. The chart provided on the Thoroughfare Map shall be used during the development process to calculate the minimum required right-of-way (ROW) dedication and the width of pavement for Roadway Adequacy Fees. In addition to the recommended right-of-way listed in the cross sections, additional right-of-way will be needed at certain intersections to accommodate turn lanes.

The right-of-way width and cross section elements shall ultimately be determined at the time of design. The roadway should be based on the adjacent land uses and recommendations of this plan.

The following roadway classifications are used in this plan and map.

- ◆ Arterial Roadways
- ◆ Collector Roadways
- ◆ Local Roadways
- ◆ Alleys



ARTERIAL ROADWAYS

Arterials primarily provide for traffic movement, with a minor function of providing direct access to abutting property. Arterials typically serve as connections between major traffic generators and land use concentrations, and facilitate large volumes of through traffic traveling across a community. Because direct access to abutting property is a secondary function of arterial roadways, access should be carefully managed to avoid adverse impacts traffic flow on these facilities.

Cross section elements are defined in the table on the Thoroughfare Map.

Roadway Features:

- ◆ Minimum Right-Of-Way: As defined in Thoroughfare Map Table
- ◆ A minimum of 20' of additional right-of-way shall be dedicated for a length of 200' prior to intersecting an arterial roadway to accommodate turn lanes.
- ◆ Minimum Number of Lanes: Typically 4 lanes with medians
- ◆ On-Street Parking: Not permitted
- ◆ Driveways: Most driveways are right in right out with median breaks and left turn lanes spaced in accordance with the Austin Transportation Criteria Manual
- ◆ Screening Wall: Required if residential homes side or back up to roadway – Masonry
- ◆ Pedestrian and Bicycle Facilities: Hike and Bike trails and sidewalks shall be provided
- ◆ Pedestrian Facilities: Bulb-outs/pedestrian refuges may be provided at some intersections to reduce the crossing distance
- ◆ Transit Facilities: Bus routes will be determined in the future

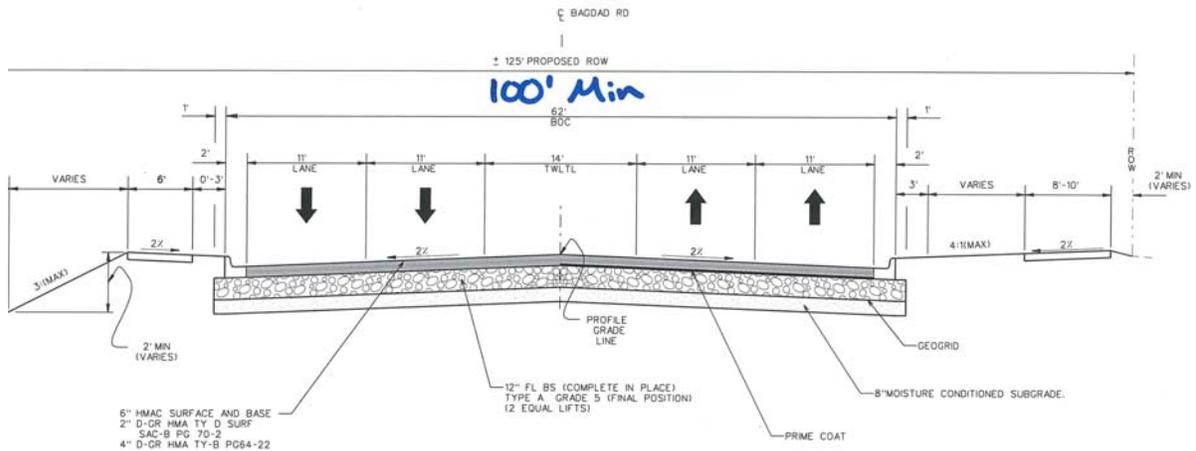
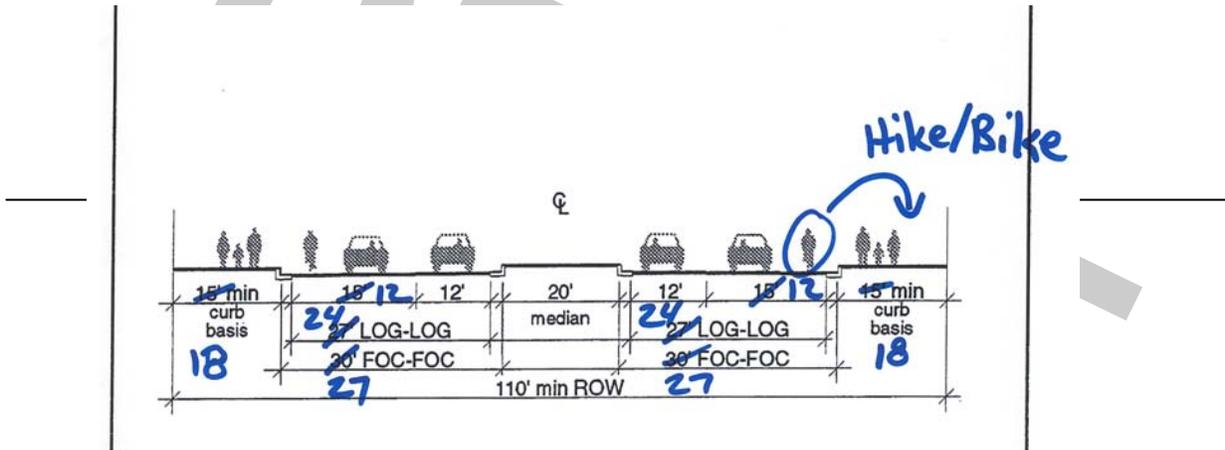


TRANSPORTATION PLAN

Typical Arterial Cross Sections:

DRAFT CROSS SECTIONS

To be updated



TRANSPORTATION PLAN

COLLECTOR ROADWAYS

Collector roadways provide for a balance of traffic movement and property access functions. Traffic movement is often internal to localized areas, with collectors connecting residential neighborhoods, parks, churches, etc. with the arterial system and commercial areas. These roadways tend to carry a high volume of traffic over a much shorter distance while providing for land access. As compared to arterial roadways, collectors accommodate smaller traffic volumes over shorter distances.

The City has five typical collectors:

- ◆ Residential Collector 60'
- ◆ Residential Collector 64'
- ◆ Neighborhood Collector
- ◆ Primary Commercial Collector
- ◆ Secondary Commercial Collector





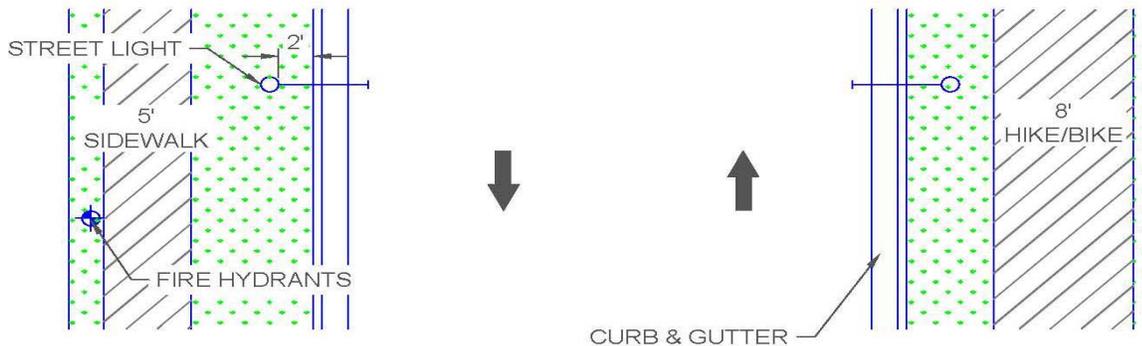
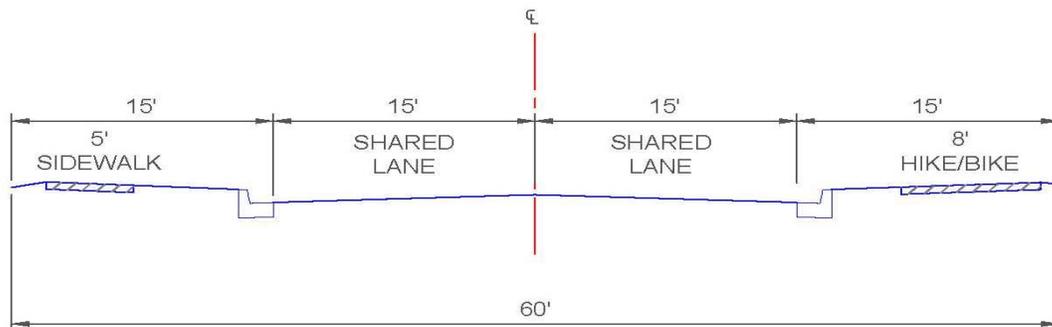
TRANSPORTATION PLAN

RESIDENTIAL COLLECTOR 60'

General Description: A residential collector is a low volume roadway that is typically internal to the subdivision or connecting the subdivision.

Roadway Features:

- ◆ Minimum Right-Of-Way: 60'
- ◆ Maximum Number of Lanes: 2 lanes
- ◆ Minimum Lane Width: 15'
- ◆ Sidewalks: 8' Hike and Bike and a 5' Sidewalks
- ◆ On-Street Parking: Limited
- ◆ Driveways: Residential driveways are not permitted.
- ◆ Screening Wall: Required if residential homes side or back up to roadway – Masonry or concrete panel
- ◆ Bicycle Facilities: Shared lanes due to low volume of traffic
- ◆ Intersections: Pavement and ROW may widen to provide left turn lanes at intersections with other collectors or entrances to subdivisions, schools, amenity centers, etc. A minimum of 10' of additional right-of-way shall be dedicated for a length of 200' prior to intersecting an arterial roadway to accommodate left turn lanes. This assumes the right lane is a right turn/through lane.



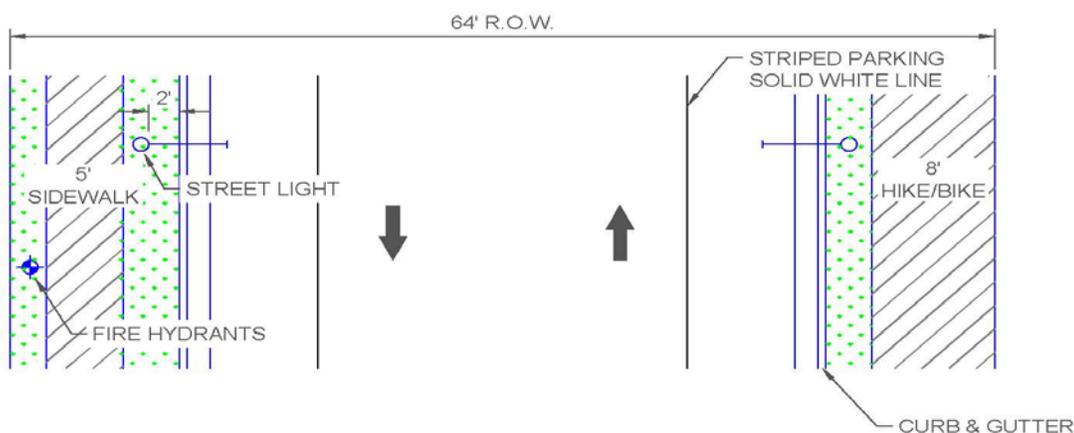
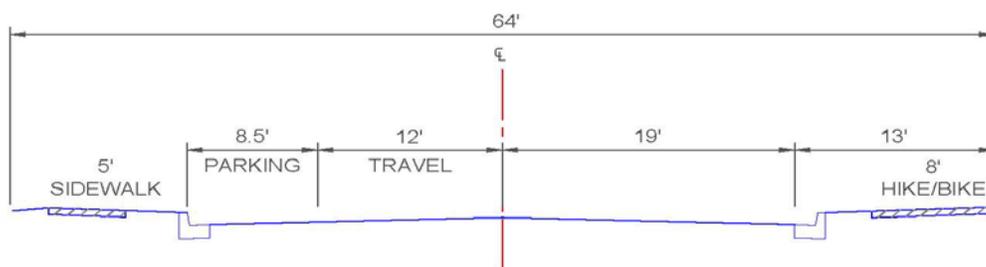
TRANSPORTATION PLAN

RESIDENTIAL COLLECTOR 64'

General Description: A residential collector is a low volume roadway that is typically internal to the subdivision or connecting the subdivision. This type of collector is for use when on street parking is necessary such as when adjacent to alley loaded homes, schools, or amenity centers fronting the collector.

Roadway Features:

- ◆ Minimum Right-Of-Way: 64'
- ◆ Maximum Number of Lanes: 2 lanes
- ◆ Minimum Lane Width: see cross section
- ◆ On-Street Parking: Permitted. On street parking shall end to accommodate left turn lanes at intersections with other collectors or entrances to subdivisions, schools, amenity centers, etc.
- ◆ Bulb-outs: Used to break up the on-street parking and to prevent parking near intersections
- ◆ Screening Wall: Not typical as cross section is intended to be used when facilities are fronting the roadway
- ◆ Sidewalks: 8' Hike and Bike and a 5' Sidewalks
- ◆ Bicycle Facilities: Bicycle boxes may be provided adjacent to on street parking
- ◆ Intersections: Pavement and ROW may widen to provide left turn lanes at intersections with other collectors or entrances to subdivisions, schools, amenity centers, etc. A minimum of 10' of additional right-of-way shall be dedicated for a length of 200' prior to intersecting an arterial roadway to accommodate left turn lanes.





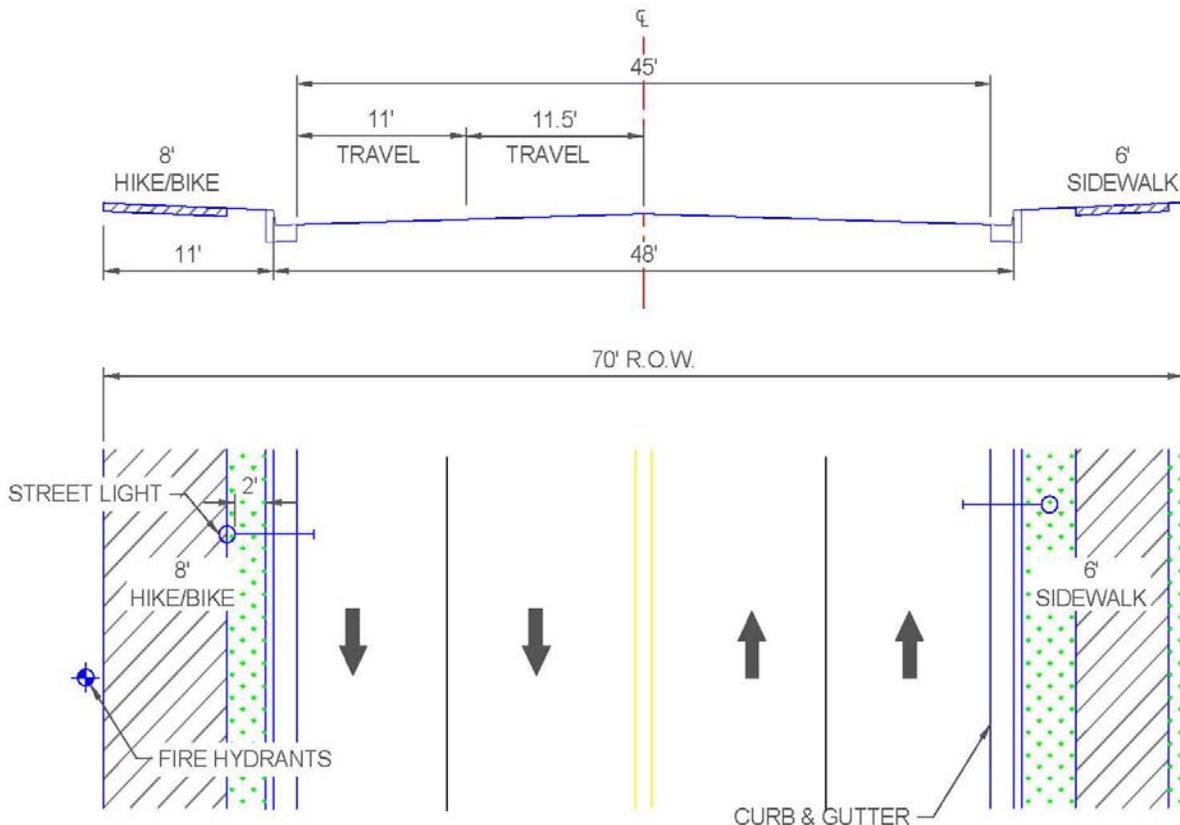
TRANSPORTATION PLAN

NEIGHBORHOOD COLLECTOR

General Description: A neighborhood collector is a low volume roadway that typically connects multiple subdivisions and compatible uses such as churches, schools and local commercial. This is a four lane undivided roadway. The additional lanes accommodate cars turning from the through lanes while allowing traffic to continue.

Roadway Features:

- ◆ Minimum Right-Of-Way: 70'
- ◆ Maximum Number of Lanes: 4 Lanes
- ◆ Minimum Lane Width: see cross section
- ◆ On-Street Parking: Not permitted.
- ◆ Screening Wall: Required if residential homes side or back up to roadway – Masonry or concrete panel
- ◆ Sidewalks: 8' Hike and Bike and a 5' or 6' Sidewalks
- ◆ Bicycle Facilities: Hike and Bike trails shall be provided to accommodate bicyclists
- ◆ Intersections: Pavement and ROW may widen to provide left turn lanes at intersections with other collectors or entrances to subdivisions, schools, amenity centers, etc. A minimum of 10' of additional right-of-way shall be dedicated for a length of 200' prior to intersecting an arterial roadway to accommodate left turn lanes.



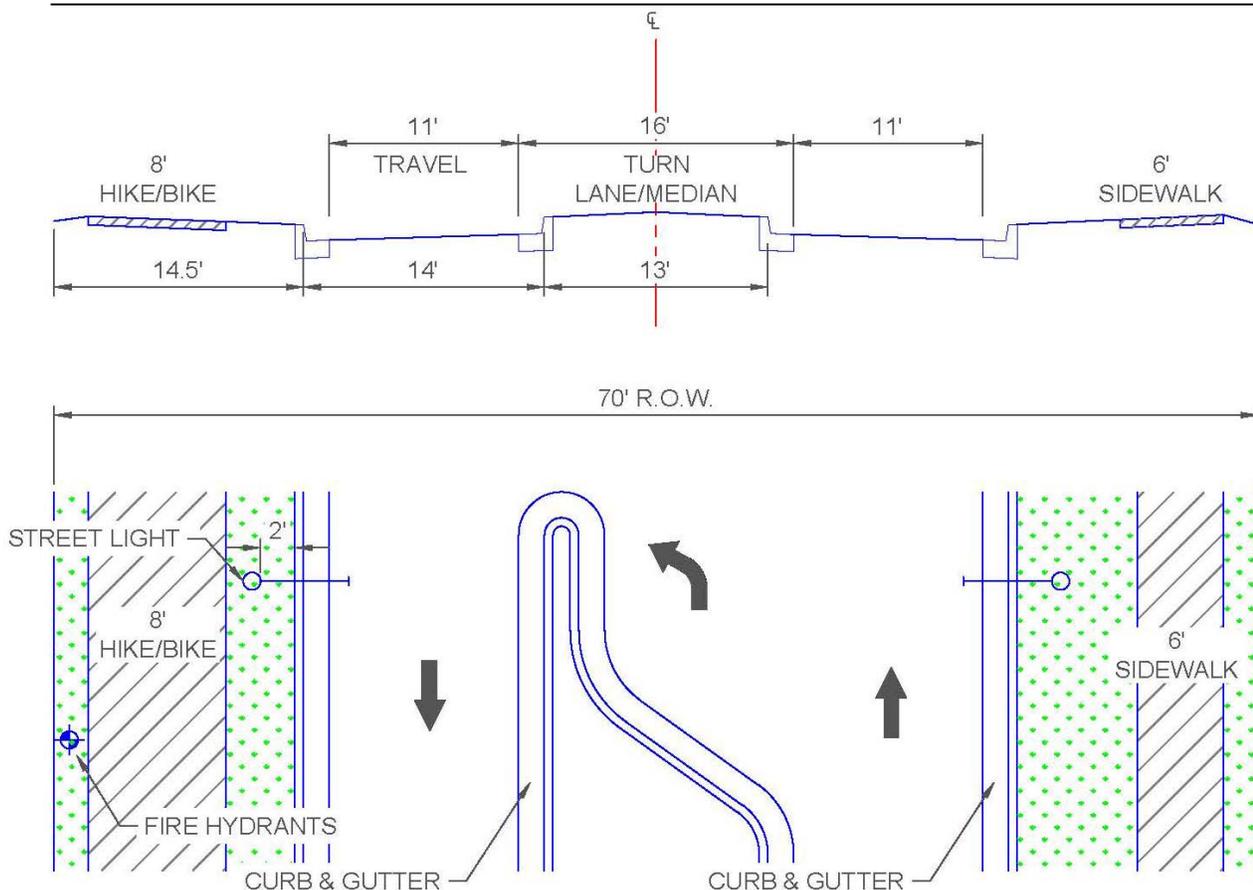
TRANSPORTATION PLAN

PRIMARY COMMERCIAL COLLECTOR

General Description: A primary commercial/industrial collector is a long commercial or industrial roadway. This type of collector provides connectivity between commercial/industrial uses as well as connecting neighborhoods with commercial uses.

Roadway Features:

- ◆ Minimum Right-Of-Way: 70 feet
- ◆ Maximum Number of Lanes: 2 lanes and a left turn lane with a median
- ◆ Minimum Lane Width: 11 feet
- ◆ On-Street Parking: Not permitted
- ◆ Driveways: Most driveways are right in right out with median breaks and left turn lanes spaced in accordance with the Austin Transportation Criteria Manual
- ◆ Screening Wall: Required if residential homes side or back up to roadway – Masonry or concrete panel
- ◆ Sidewalks: 8' Hike and Bike and a 6' Sidewalk
- ◆ Bicycle Facilities: Hike and Bike trails shall be provided to accommodate bicyclists
- ◆ Intersections: A minimum of 10' of additional right-of-way shall be dedicated for a length of 200' prior to intersecting an arterial roadway to accommodate right turn lanes.





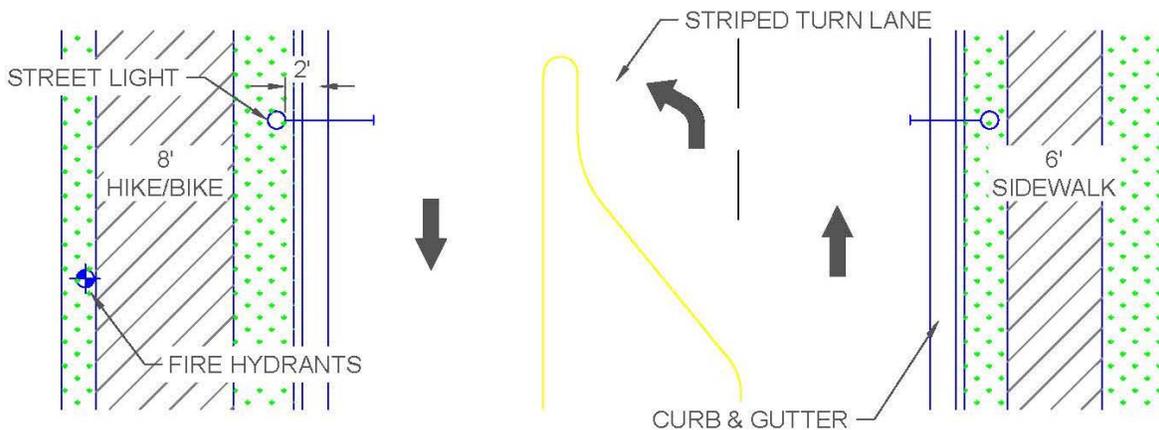
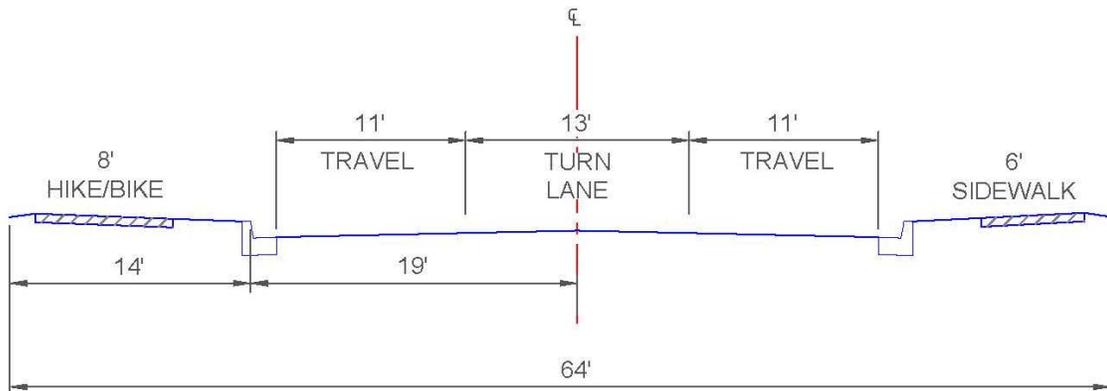
TRANSPORTATION PLAN

SECONDARY COMMERCIAL COLLECTOR

General Description: A secondary commercial/industrial collector is a short commercial or industrial roadway. This type of collector provides connectivity between commercial/industrial uses as well as connecting neighborhoods with commercial uses.

Roadway Features:

- ◆ Minimum Right-Of-Way: 64'
- ◆ Maximum Number of Lanes: 2 lanes and a continuous two way left turn lane
- ◆ Minimum Lane Width: 11 feet
- ◆ On-Street Parking: Not permitted
- ◆ Screening Wall: Required if residential homes side or back up to roadway – Masonry or concrete panel
- ◆ Sidewalks: 8' Hike and Bike and a 6' Sidewalk
- ◆ Bicycle Facilities: Hike and Bike trails shall be provided to accommodate bicyclists
- ◆ Intersections: A minimum of 10' of additional right-of-way shall be dedicated for a length of 200' prior to intersecting an arterial roadway to accommodate right turn lanes.



TRANSPORTATION PLAN

LOCAL ROADWAYS

Local roadways provide access to adjoining residential land uses and distribute traffic to the surrounding roadway network. Most of the other roadways within the City of Leander that are not listed in any of the other descriptions are local roadways. The character of residential roadways can change from neighborhood to neighborhood. It should be noted that the roadways are not entirely for vehicular movement and are intended to accommodate pedestrians and bicyclists.

Not every typical local roadway cross section has been identified in the Plan. The cross sections will vary based on the character of the surrounding area and land uses. An example of a typical residential local road section is provided.

Developments subject to the *SmartCode* should follow standards set in Chapter 3 of the *SmartCode*.





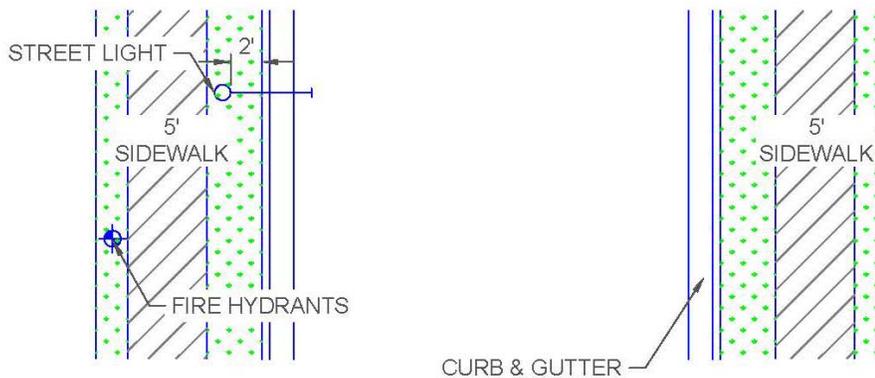
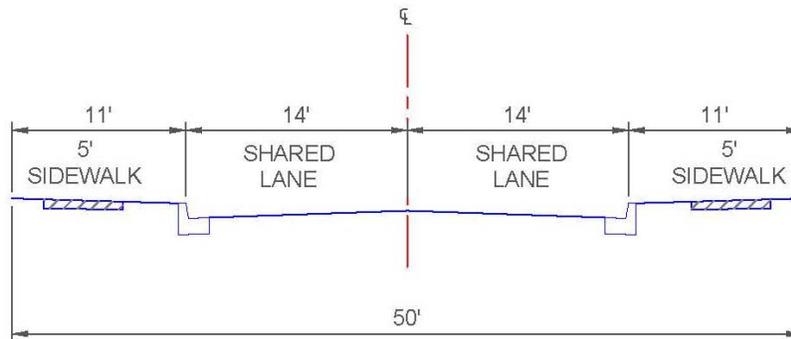
TRANSPORTATION PLAN

RESIDENTIAL SUBURBAN LOCAL ROADWAY:

General Description: Residential local roadways provide access to single family residential homes. These roadways serve pedestrians, bicyclists, and vehicles.

Residential Suburban Local Roadway Features:

- ◆ Minimum Right-Of-Way: 50'
- ◆ Maximum Number of Lanes: 2 lanes
- ◆ Minimum Lane Width: 14' including the gutter
- ◆ Sidewalks: 4' minimum and 5' recommended
- ◆ On-Street Parking: Permitted; through traffic can have a yield condition
- ◆ Bicycle Facilities: On street





TRANSPORTATION PLAN

ALLEYS

Alleys run parallel to roadways and provide rear access to residential lots with rear entry garages or commercial buildings.

Old Town has several existing alleys. As per the *Smart Code*, new developments may utilize these existing alleys without improving or paving the alley.

Alleys must be designed to accommodate trash trucks. Attention should be given to curb returns for alley to alley intersections.

Two types of alleys are often proposed: Residential Alleys and Commercial Alleys.

Residential Alley Features:

- ◆ Minimum Right-Of-Way: 20'
- ◆ Intended to provide access to rear entry garages
- ◆ Minimum Pavement Width: 14' of pavement and 3 feet of decomposed granite on each side
- ◆ Paving Materials: Public alleys are required to be concrete and private alleys may be concrete or asphalt. Asphalt alleys must include concrete ribbon curbs in the 14'.

Commercial Alley Features:

- ◆ Intended to provide access to commercial buildings in an urban area for fire protection, deliveries, unloading, etc.
- ◆ Minimum Right-Of-Way: 20'
- ◆ Minimum Pavement Width: 20' of pavement
- ◆ Paving Materials: Alleys are required to be concrete if directly adjacent to a building.



PEDESTRIAN & BICYCLE FACILITIES PLAN

The Pedestrian and Bicycle Facility Plan provides a plan for non-motorized transportation within the City. This plan will provide for alternatives to vehicular transportation and promote connectivity throughout the City. In addition, these facilities will provide opportunities for people that cannot or do not wish to drive vehicles to travel safely to their destination.

Pedestrians

- ◆ All new roadways, except rural local roads, shall have sidewalks and/or hike and bike trails.
- ◆ Sidewalks on local residential roadways will be five feet wide.
- ◆ Sidewalks in commercial areas will be six feet wide.

Bicyclists

- ◆ The Transportation Plan strives to provide alternatives to riding in the roadway.
- ◆ All arterial roadways include 10' hike and bike trails.
- ◆ Most collectors include a minimum of 8' hike and bike trails.
- ◆ Bicyclists share the road with cars in residential areas and on local roadways.

Hike and Bike Trails are usually along roadways and creeks. These facilities are typically constructed as the adjacent area is developed, though some are capital improvement projects through the Parks Department.

Most hike and bike trails in Leander will be shared by pedestrians and bicyclists. Sidewalks are intended for pedestrians. Major paths, such as those connecting the rail station to ACC, have parallel hike and bike trails which will reduce the traffic volume per trail. In the future shared facilities can be striped to divide pedestrians and bicyclist if the volume of traffic warrants it.

The Hike & Bike Map visually identifies the desired network of pedestrian and bicycle facilities.



THOROUGHFARE MAP

The Thoroughfare Map on the next page provides roadway classifications for the roadways located with the City of Leander jurisdiction. The map also identifies the locations of the existing and proposed roadways throughout the City.

DRAFT

THOROUGHFARE PLAN ROW, SIDEWALKS, TRAILS & CROSS SECTIONS²

ROADWAY	SEGMENT	PROPOSED ROW (Min) ³	PROP. PAVEMENT CROSS SECTION ⁴	PROPOSED TRAILS & SIDEWALKS			
				N	S	E	W
TOLLWAY							
183A	All	Existing	Tollway & Frontage			10'	10'
MAJOR ARTERIAL							
A1	All	82'	54'	10'	6'		
A2	All	110'	2 @ 27'	8'	6'		
Bagdad Rd	S City Limits to Old 2243 W	Existing	Existing			6'	8'
Bagdad Rd / CR 279	Old 2243 W to CR 280	100'	66'			10'	6'
Bagdad Rd / CR 279	CR 280 to N ETJ Limits	100'	66'			8'	6'
CR 175	All	90'	2 @ 24'			8'	6'
Crystal Falls Pkwy	Lakeline Blvd to US 183	Existing	Existing	8'	6'		
E Crystal Falls Pkwy	US 183 to CR 175	110'	2 @ 27'	8'	6'		
Hero Way	Inside TOD	TOD	TOD	10'	6'		
Hero Way	TOD boundary to RM 2243	110'	2 @ 27'	8'	6'		
Old 2243 W / Nameless Rd	1,000' W of Lakeline Blvd to US 183	100'	66'	10'	6'		
	RM 1431 to 1,000' W of Lakeline Blvd	82'	54'	6'	10'	10'	6'
Lakeline Blvd	S City Limits to Crystal Falls Pkwy	Existing	Existing			6'	6'
	Crystal Falls Pkwy to Old 2243 W	90'	2 @ 24'			10'	6'
	Old 2243 W to W San Gabriel Pkwy	90'	2 @ 24'			10'	6'
Mel Mathis Ave	RM 2243 to San Gabriel Pkwy	TOD	TOD			22'	22'
Ronald W Reagan Blvd	All	Existing	Existing			10'	10'
RM 2243	Hero Way to E ETJ	110'	2 @ 27'	6'	10'		
San Gabriel Pkwy	Nameless Rd to Ronald W Reagan Blvd	110'	2 @ 27'	6'	10'		
SH 29	All	Existing		6'	10'		
US 183	All	Existing / TOD	Existing / TOD			10'	6'
Osage Dr	Durango Hills Dr to FM 1431	90	2 @ 24'	5'	5'		
COLLECTOR⁵							
Bryson Ridge Trl	All	Varies Min of 70'	Varies Min 2 @ 18'	8'	-		
Broade St	Commercial Portion	70'	45'			8'	6'
	Residential Portion	60'	37'			8'	6'
C1	All	60'	37'	8'	6'		
C7	All	70'	45'			8'	6'
C11/Durango Hills Dr	All	60'	37'	5'	5'		
C13	All	TOD	TOD	TOD	TOD	TOD	TOD
Collaborative Way	All	64'	Existing	8'	6'		
CR 176	All	60'	37'			8'	5'
CR 269	All	64'	41'			8'	6'
CR 270	All	70'	38'			8'	6'
CR 280	All	64'	41'	8'	6'		
CR 281	All	60'	37'	6'	8'		
Crystal Falls Pkwy	Lakeline Blvd to Crystal Falls Golf Club	Existing	Existing				
E Crystal Falls Pkwy/CR 177	Ronald W Reagan Blvd to E City Limits	64'	41'	8'	6'		
East St	All	TOD	TOD	TOD	TOD	TOD	TOD
Gabriels Horn Rd	All	60'	37'	5'	5'	5'	5'
Good Night Trl	All	Existing	Existing	5'	5'		
Halsey Dr	South of Collaborative Way	70'	Existing			6'	6'
	North of Collaborative Way	60'	37'			6'	6'
Hazelwood St	All	64'	41'	6'	6'		
Heritage Grove Rd	All	60'	37'	8'	6'		
Horizon Park Blvd	E Crystal Falls Pkwy to RM 2243	64'	41'			6'	8'
Horizon Park Blvd	McCarthy Dr to E Crystal Falls Pkwy	Existing	Existing			5'	5'
Journey Pkwy	Ronald W Reagan Blvd to Sam Bass Rd	70'	Existing	6'	8'		
Kauffman Loop	All	70'	45'	8'	6'		
Lakeline Blvd	San Gabriel Pkwy to CR 281	90'	2 @ 18'			10'	6'
Leander Dr	All	64'	41'			6'	6'
Logan Del Way	All	60'	37'			4'	4'
Mel Mathis Ave	San Gabriel Pkwy to US 183	70'	45'			8'	8'
McCarthy Dr	All	Existing	Existing	4'	4'		
Metro Dr	All	70'	45'	8'	6'		
Municipal Dr	Bagdad Rd to Northern Trl	Existing	Existing	6'	6'		
	Northern Trl to West St	Existing	Existing	6'	6'		
Osage Dr	Outlook Ridge Lp to US 183	Existing	Existing	5'	5'		
	Outlook Ridge Lp to Durango Hills Dr	70'	37'	5'	5'		
Palmera Ridge Blvd	All	64'	41'	4'	4'		
Pleasant Hill Rd	All	60'	37'			5'	5'
Raider Way	E Woodview Dr to RM 2243	64	41'			6'	8'
	183A to E Woodview Dr	70'	45'			6'	8'
RM 2243	N East St to Hero Way	70'	45'	8'	6'		
Sonny Dr	West Dr to Leander Dr	Existing	Existing	6'	6'		
	Lakeline Blvd to West Dr	64'	41'	6'	6'		
South St (East & West)	All	Existing	Existing	6'	6'		
South Brook Dr	All	60'	37'	6'	6'		
Sunny Brooke Dr	All	60'	37'			6'	6'
Travisso Pkwy	All	Varies Min of 100'	Varies Min of 2 @ 24'	8'	8'	8'	8'
Via De Sienna Blvd	All	60'	37'	8'	6'		
Vista Ridge Dr	Lakeline Blvd to Bagdad Rd	64'	41'	6'	6'		
Waterfall Ave	All	60'	37'	6'	6'	6'	6'
West Dr	All	64'	41'	6'	6'		
E Woodview Dr	All	70'	45'	8'	6'		

- 1 - Table reflects ultimate road section. Existing roads may or may not conform to ultimate section.
- 2 - Streets, sidewalks and trails in TOD to follow Transect Plan and Smart Code. Street alignments subject to adjustment.
- 3 - Increase ROW width within 300' of intersection of arterials by 15' with appropriate transition to provide for right turn lane. Additional ROW and pavement shall be provided as necessary to match section of opposite streets at intersections. ROW width may be altered to accommodate drainage or as determined appropriate by the City Engineer.
- 4 - Measurements are to lip of gutter. Pavement sections subject to modification by City Engineer.
- 5 - Additional collector streets required as necessary in accordance with the Transportation Criteria Manual for projected traffic and land use.

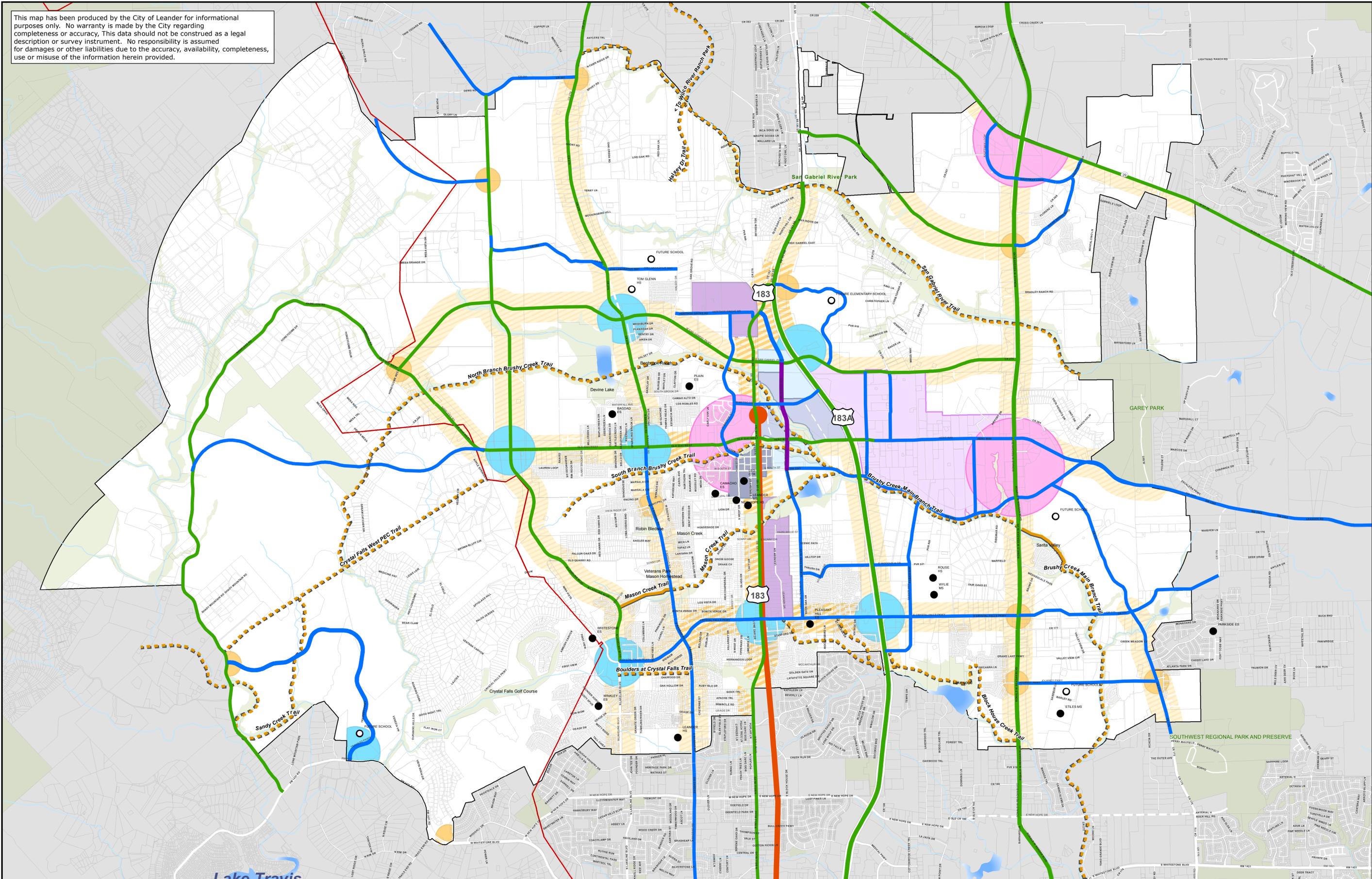
6 - Width and location of sidewalks and trails to comply with requirements of Parks, Recreation & Open Space Master Plan & Composite Zoning Ordinance

NOTES:

Where the proposed ROW, sidewalk and pavement result in less than 4' of landscape/lawn area per side of roadway behind the curb, additional ROW should be dedicated to meet the 4' minimum width lawn area. In lieu of additional ROW, commercial tracts can place the sidewalk in a pedestrian access easement and residential developments can provide a landscape lot parallel to the ROW.

The chart above should be used to calculate fees when a development chooses to pay a fee in-lieu of constructing a roadway; however, the actual cross section for roads to be constructed should be determined based on adjacent land uses and the recommendations of Appendix E of the Comprehensive Plan.

This map has been produced by the City of Leander for informational purposes only. No warranty is made by the City regarding completeness or accuracy. This data should not be construed as a legal description or survey instrument. No responsibility is assumed for damages or other liabilities due to the accuracy, availability, completeness, use or misuse of the information herein provided.



CITY OF LEANDER, TEXAS

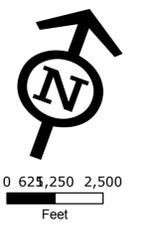
HIKE & BIKE MAP DRAFT JULY 21, 2016

HIKE & BIKE

- 8
- 10
- 22
- Secondary Trail
- Secondary Trail

FUTURE LAND USE

- Open Space
- Community Center
- Old Town Mixed Use
- Mixed Use Corridor
- Activity Center
- Employment Mixed Use
- Commercial Corridor
- Transit Supportive Mixed Use
- Industrial District
- Neighborhood Center
- Station Area Mixed Use





Minutes

**PLANNING & ZONING COMMISSION WORK SESSION
CITY OF LEANDER, TEXAS
Pat Bryson Municipal Hall ~ 201 North Brushy Street
Leander, Texas**

Thursday ~ July 28, 2016 at 6:00 pm

**Place 1 Chris Schwendenmann
Place 2 Morgan Cotten
Place 3 Jason Anderson
Place 4 Sid Sokol - Chair**

**Place 5 Richard Allen – Vice Chair
Place 6 Angela Means
Place 7 Marshall Hines**

This meeting is open to the Public but does not allow for public participation. No action will be taken by the Planning and Zoning Commissioners regarding City business before, during or after this meeting

1. Open Work Session
Vice Chair Allen opened the work session at 6:05 pm
2. Roll Call
**All Commissioners were present except Chair Sokol, Commissioner Anderson, and Commissioner Cotten.
Chair Sokol arrived at 6:10 p.m. and took his seat.
Commissioner Cotten arrived at 6:55 p.m. and took his seat.**
3. Transportation Plan Discussion
 - a. Staff Presentation (10 minutes)
Terri Crauford, Assistant City Engineer, presented the draft of the Transportation Plan.
 - b. Thoroughfare Map and Cross Sections (15 minutes)
 - c. Pedestrian & Bicycle Facilities Plan and Map (15 minutes)
 - d. General Discussion (10 minutes)
The Planning & Zoning Commission discussed the draft of the Transportation Plan.
 - **Staff needs to consider intersection controls such as roundabouts.**

- Provisions should be included for right turn lanes with regard to encouraging the traffic to not go straight and to turn right.
- The entire community needs to be considered with this plan including pedestrians, cyclists, and people with disabilities.
- Wider travel lanes encourage higher speeds, the extra pavement width should be used for on street cyclist facilities. Consider providing on-street bike lanes for higher speed cyclists and wider sidewalks/hike and bike trails for slower speed cyclists.
- Provide data to support the need for bicycle lanes on the street.
- The City should consider adopting requirements for cyclist visibility such as flags or blinking lights.
- Sidewalk construction should be part of the construction of all new roadways.
- Sidewalk construction should not be deferred until site development for projects other than single-family.
- The timing for pedestrian crosswalk signals should be evaluated.
- Sidewalk alignment should be rational and provide for direct connections to the crossing, reduce curvy sidewalks close to intersections.
- Provide refuge areas in the median breaks for pedestrians.
- Include provisions that will require landscaping in medians along arterial and collector roadways.
- Plan for street trees.

4. Adjournment at **7:05 pm**

AGENDA ITEM # 5



Executive Summary

September 15, 2016

Subject Presentation from ACC on current status of the San Gabriel campus master plan.

Background: ACC will present the current status of the San Gabriel (Leander) campus master plan including a presentation of the proposed architectural theme of the campus.

Financial Consideration: None for this item.

Recommendation: This is a discussion item.

Attachments: None

Prepared by: Tom Yantis, Assistant City Manager